



**WATFORD  
BOROUGH  
COUNCIL**

# **DEVELOPMENT MANAGEMENT COMMITTEE**

**7 September 2021**

**7.00 pm**

**Town Hall, Watford**

**Contact**

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# Committee Membership

Councillor P Jeffree (Chair)

Councillor R Martins (Vice-Chair)

Councillors N Bell, S Johnson, J Pattinson, A Saffery, R Smith, S Trebar and M Watkin

## Agenda

### Part A – Open to the Public

#### CONDUCT OF THE MEETING

The committee will take items in the following order:

1. All items where people wish to speak and have registered with Democratic Services.
2. Any remaining items the committee agrees can be determined without further debate.
3. Those applications which the committee wishes to discuss in detail.

#### 1. **Apologies for absence**

#### 2. **Disclosure of interests**

#### 3. **Minutes**

The [minutes](#) of the meeting held on 27 July 2021 to be submitted and signed.

#### 4. **21/00410/FULM 46 And 48-50 High Street Watford WD17 2BS (Pages 5 - 45)**

#### 5. **21/00698/FULM - Marchwood House, 934 - 974 St Albans Road, Watford, WD25 9NN (Pages 46 - 91)**

#### 6. **21/00934/VARM - Hannay House, 37-39 Clarendon Road, Watford WD17 1JA (Pages 92 - 118)**

## **Introduction**

Please note that the officer report is a summary of the issues including representations made and consultation responses. Full details of the applications, plans submitted, supporting information and documents, representations made, consultation responses and correspondence can be found on the council's web based [Public Access system](#) using the application reference or address.

Specific policy considerations for each application are detailed within the individual reports. The background papers and policy framework listed below have been relied upon in the preparation of the reports in this agenda.

## **Background papers**

- The current planning applications under consideration and correspondence related to that application.
- All relevant third party representations and consultation replies received.

## **Policy Framework**

- The Statutory Plans and Supplementary Planning Guidance, together with relevant Government legislation, Circulars, Advice, Orders, Directions and Guidance listed below:

## **Local Planning Documents**

Local Development Documents provide the framework for making planning decisions. These can be found on the Council's [website](#) and include:

- the existing Local Plan which consists of the Core Strategy, saved policies in the Watford District Plan 2000 and Proposals Map); and
- Supplementary Planning Documents.

## **County Planning Documents**

The Hertfordshire Waste Local Plan and Minerals Local Plan prepared by Hertfordshire County Council are material considerations alongside the Watford Local Plan. These documents can be found on the county council's [website](#).

## **National Planning Documents**

Key legislation can be found using this [weblink](#), including:

- Growth and Infrastructure Act (2013)
- Housing and Planning Act (2016)

- Localism Act (2011) and subsequent amendments
- Planning Act (2008) and subsequent amendments
- Planning and Compulsory Planning Act (2004) and subsequent amendments
- Town and Country Planning Act (1990) and subsequent amendments
- Town and Country Planning (Local Planning) (England) Regulations 2012 and subsequent amendments.

National guidance can be found on the government service and information [website](#), including:

- National Planning Policy Framework (revised February 2019) and supporting Technical Guidance
- Planning Practice Guidance (PPG) (web based)
- Planning policy for traveller sites
- Relevant government circulars
- Relevant Ministerial Statements (which will be referred to in the individual reports as necessary)

### **Section 106 Planning obligations and Community Infrastructure Levy (CIL)**

The Council introduced the Community Infrastructure Levy (CIL) with effect from 1 April 2015. The CIL charge covers a wide range of infrastructure as set out in the Council's Regulation 123 list, including highways and transport improvements, education provision, youth facilities, childcare facilities, children's play space, adult care services, open space and sports facilities. CIL is chargeable on the relevant net additional floorspace created by the development. The charge is non-negotiable and is calculated at the time that planning permission is granted where relevant. Section 106 planning obligations can only be used to secure affordable housing provision and other site specific requirements, such as the removal of entitlement to parking permits in Controlled Parking Zones and the provision of fire hydrants.

### **Human Rights implications**

The Local Planning Authority is justified in interfering with the applicant's human rights in order to alleviate any adverse effect on adjoining properties and their occupiers and on general public amenity. With regard to any infringement of third party human rights, these are not considered to be of such a nature and degree as to override the human rights of the applicant and therefore warrant refusal of planning permission.

<b>Committee date</b>	Tuesday, 7 <sup>th</sup> September 2021
<b>Application reference</b> <b>Site address</b>	21/00410/FULM 46 And 48-50 High Street Watford WD17 2BS
<b>Proposal</b>	Demolition of existing buildings and redevelopment of a mixed-use building over part 4, part 5 and part 8 storeys including flexible commercial floor space under Class E (shop, cafe, professional/ financial services) and Sui Generis (public house/ drinking establishment) at the ground floor fronting High Street, with residential accommodation (C3) above for 25no. dwellings, with rear access from Wellstones. Additional highway improvements to Wellstones with installation of new pedestrian footpath.
<b>Applicant</b>	46 and 48-50 High Street Limited
<b>Agent</b>	Nissen Richards Studio
<b>Type of Application</b>	Full Planning Permission
<b>Reason for committee Item</b>	Major Development
<b>Target decision date</b>	10 <sup>th</sup> September 2021
<b>Statutory publicity</b>	Site notice, paper advertisement and neighbour letters
<b>Case officer</b>	Alice Reade, <a href="mailto:alice.reamde@watford.gov.uk">alice.reamde@watford.gov.uk</a>
<b>Ward</b>	Central

## 1. Recommendation

- 1.1 That planning permission be granted subject to conditions, as set out in section 8 of this report.

## 2. Site and surroundings

- 2.1 The site is located on the High Street within the main town centre of Watford between the junctions with Clarendon Road to the north-west and Market Street to the south-east. It is an elongated and irregular shaped site with an area of 0.06 hectare. The site is within the designated prime retail frontage of the town centre in the Watford District Plan 2000 and is within the Special Policy Area for the Town Centre (SPA1) of the Watford Local Plan Core Strategy which seeks for a balanced provision of town centre facilities and infrastructure throughout the town centre.
- 2.2 The site frontage to High Street comprises 2 locally listed buildings (nos. 46 and 48-50, High Street) which form part of a wider group of locally listed and statutorily listed buildings (Nos 44, 46, 48-50, 54, 56 and 58) which have

created a group value to the character of this part of the High Street. Both buildings are 3 storey and in commercial use.

- 2.3 This section of High Street is pedestrian friendly with restricted access for buses and delivery vehicles only and has recently undergone significant environmental enhancements. The site backs onto Wellstones to the rear which is a public highway which has historically operated as a rear service road to properties in High Street. It is one-way (north to south), single carriageway and currently has very limited extents of pedestrian footpath. To the west of Wellstones is the Telephone Exchange site comprising a large exchange building and extensive surface level car parking.
- 2.4 Adjoining the site to the south-east is a recently completed mixed-use development including 2 commercial units fronting High Street and 56 flats (52A-56, High Street). This development has a building of 4 storeys on High Street and 7 storeys on Wellstones. A similar scale of development has also recently been approved at 60, High Street adjoining no. 52A-56 (latest ref 21/00014/FULM) for a commercial unit fronting High Street and 32 dwellings in a building of 4 storeys fronting onto the High Street and rising to 8 storeys onto Wellstones.
- 2.5 The immediate context of the site also includes the recently completed shopping centre extension at Charter Place on the opposite side of High Street which includes retail, restaurant and leisure uses (including an Imax cinema and bowling).
- 2.6 In addition to the locally listed buildings on the site, there are adjacent locally listed buildings and statutorily listed buildings within the context including the Grade II Listed former bank at 58 High Street (now Five Guys restaurant). The site is not within a Conservation Area.

### **3. Summary of the proposal**

#### **3.1 Proposal**

- 3.2 The proposal is for the demolition of the existing building on site and the erection of a development as follows:
  - i) Mixed use development to include a 4 storey building fronting High Street (north block) and a part 5 and part 8 storey building with access from Wellstones (south block). Blocks to be linked at ground floor with a first floor podium level above.
  - ii) Development to contain 25 apartments comprising:

- 16 no 1 bed units
  - 9 no 2 bed units
- iii) Two ground floor units fronting the High Street of 212sqm and 188sqm (GEA) for flexible commercial floor space under Class E (shop, cafe, professional/ financial services) and Sui Generis (public house/drinking establishment).
- iv) Bin and cycle storage at ground floor level with access from Wellstones.
- v) Highway improvements to Wellstones with installation of new pedestrian footpath on the north-eastern side.

### 3.3 **Conclusion**

The town centre site is considered as a central and sustainable location suitable for higher density development. The development of 4 storeys onto the High Street and 8 storeys onto Wellstones would be of appropriate height and massing for the site and surroundings, in line with the base heights set out in the emerging Tall Buildings policy within the draft Local Plan, and would relate successfully to the similar approved developments at Nos. 54A-56 and No. 60 High Street.

- 3.4 The layout, massing and design has been successfully resolved through a constructive pre-application process to address previous officer concerns and comments from the Watford Place Shaping Panel. Despite the significant constraints of the site, the proposed layout and design create a development of high quality in its visual appearance and the residential accommodation it provides. The loss of the locally listed buildings is considered to be justified by the good design of the scheme and the wider planning benefits the development offers in creating new commercial units, 25 new homes and a new footpath on Wellstones. On balance, this is considered to be a positive addition to the High Street and wider town centre.

## 4. **Relevant policies**

- 4.1 Members should refer to the background papers attached to the agenda. These highlight the policy framework under which this application is determined. Specific policy considerations with regard to this particular application are detailed in section 6 below.
- 4.2 Paragraph 11 d) of the NPPF 2021 establishes the ‘presumption in favour of sustainable development’ and the principles of the ‘tilted balance’ that apply where a local planning authority cannot demonstrate a 5 year housing supply or have failed to deliver at least 75% of their housing requirement as part of the Housing Delivery Test. Where the tilted balance applies, decision makers should grant permission unless NPPF policies on protected areas or assets of

particular importance provide a clear reason for refusing development or, any adverse impacts of granting permission would significantly and demonstrably outweigh the benefits, assessed against NPPF policies taken as a whole. The tilted balance has the effect of shifting the weight in the planning balance away from local policies and towards the NPPF.

- 4.3 The Council cannot currently demonstrate a 5 year supply of housing and scored below 75% in the most recent Housing Delivery Test results and therefore the 'tilted balance' applies to the determination of this planning application.

## **5. Relevant site history/background information**

- 5.1 Application ref 20/00365/FULM for the "demolition of existing buildings and redevelopment with a mixed-use building over part 4, part 5 and part 11 storeys including flexible commercial floorspace (A1, A2, A3 and A4) at ground floor fronting the High Street, with residential accommodation (C3) above for 38 dwellings, with rear access from Wellstones, additional highway improvements to Wellstones with installation of new pedestrian footpath" was withdrawn following officer concerns. These included:

- Insufficient design quality of the replacement building to justify loss of the locally listed buildings.
- Impact of the proposal on the adjoining heritage assets.
- Scale, footprint and elevational treatment of the 11 storey element to Wellstones.
- Poor pedestrian access from Wellstones and through the site.
- Quality of residential amenity.
- Impact on residential amenity at 52A-56, High Street.

- 5.2 The scheme has been through several iterations as part of a pre-application advice process prior to the submission of this application. This has included the review of the iterations of the proposed development by Watford's independent Place Shaping Panel by a full panel review on 18<sup>th</sup> August 2020 and then on 12<sup>th</sup> January 2021 by a Chair's review of the amended scheme. The full reports from the two panels are in Appendices 2 and 3 to this report and the findings are discussed within the report.

- 5.3 The latest pre-application advice report dated 9<sup>th</sup> December 2020 (Ref 20/01096/PREAPP) concluded that the 8 storey height was supported by officers and that the design refinements had addressed previous concerns in respect of appearance and amenity.

## 6. Main considerations

6.1 The main issues to be considered in the determination of these applications are:

- (a) Principle of development
- (b) Loss of the locally listed buildings
- (c) Layout, scale and design
- (d) Housing provision
- (e) Quality of residential accommodation
- (f) Impacts on surrounding properties
- (g) Car parking, access and transportation
- (h) Trees and landscaping
- (i) Environmental impacts
- (j) Section 106 and Community Infrastructure Levy (CIL)

### 6.2 (a) Principle of development

The Special Policy Area for the Town Centre (SPA1) of the Watford Local Plan Core Strategy applies to the site and this seeks a balanced provision of town centre facilities and infrastructure throughout the town centre. The draft Local Plan has been submitted for examination and carries only limited weight however it will also support opportunities for transformational development within the Town Centre Strategic Development Area which includes this site.

6.3 The development of residential dwellings at this site and within this area of the town centre is therefore supported in accordance with SPA1 and Policy HS1 of the Local Plan Core Strategy. The proposed replacement ground floor premises to include Class E and sui generis public house uses, maintain appropriate uses onto the High Street.

### 6.4 (b) Loss of the locally listed buildings

The development sees the loss of the locally listed buildings on the site. The current buildings provide value to the townscape by their contribution to the group of locally listed and listed buildings.

6.5 It is however considered that the loss of the buildings is justified in this instance due to the quality of the design of the replacement development and the wider planning benefits it offers in respect of high quality new homes, taking account of the tilted balance set out within the NPPF.

### 6.6 (c) Layout, scale and design

Policy UD1 of the Core Strategy sets out points to consider in achieving high quality design for new development. Development should create high quality new places and should respect and enhance the character of its area.

- 6.7 The north block of the development, consisting of a 4 storey building would be similar in scale and presence to the existing buildings it replaces and would continue the established pattern of the built form within the High Street. This new building features a traditional form however with contemporary fenestration and detailing to create a high quality building and the development would create high quality new space onto the High Street. The Place Shaping Panel advised that the northern building has been found to be of a design quality that serves to justify the removal of the existing, locally listed buildings.
- 6.8 The southern block of the development is proposed to front onto Wellstones to consist of part 5 and part 8 storeys. As informed by the Taller Buildings Study (2021) and as set out in the building height policy text of the Final Draft Local Plan, the Town Centre Strategic Development Area can comfortably accommodate buildings of a 'base height' of up to 5 storeys on the High Street, stepping up to 8 storeys to the rear. The proposed southern block of 8 storeys would be set within this base height range. Moreover, the building onto Wellstones would be similar in height to the adjacent 7 storey development and the approved development of 8 storeys at 60 High Street. The recessed and differentiated top floor of the building would ensure that the building better relates to the adjacent 7 storey building also facing Wellstones and sits comfortably within wider contextual views from the High Street.
- 6.9 The overall design language is contemporary and this has been refined to ensure that the building would be of good visual appearance despite its very narrow and irregular site boundary and layout. The façade includes good quality materials of brickwork and anodised metal. The detailing of the brickwork proposed is of similar good quality and will create a positive visual addition to the context. The use of variation on the north-west flank walls is particularly noted to improve the visual appearance of this flank elevation.
- 6.10 The layout of the development sees all dwellings accessed via the Wellstones entrance. Although this creates a long route for the access to the northern block dwellings, this layout has been refined to create a welcoming and practical route. The entrance onto Wellstones is in the form of a glazed lobby leading to a secure entrance. This creates a clear and legible entrance with good surveillance. The route through the building is now mainly open either through this lobby or via the courtyard amenity area with internal spaces

being minimised to stairs and lifts. The lobby also provides access to bin and bicycle storage.

6.11 The proposal was subject to independent design review by Watford's Place Shaping Panel on 18<sup>th</sup> August 2020 and Chair's review on 12<sup>th</sup> January 2021. The panel's reports are attached at Appendices 2 and 3. In summary, the first review (Appendix 2) found concerns with the 11 storey scheme in respect of its height and visual impact and the layout and access within the development. The scheme was amended in response to these comments and the chair's review panel (Appendix 3) identified these improvements to create a generally supported scheme in terms of scale and design.

6.12 (d) Housing Provision

The development accords with Policy HS2 of the Core Strategy and would provide an appropriate mix of dwelling sizes as follows:

<b>Dwelling type and size</b>	<b>Number</b>
1-bed 2-person	14
1-bed 2-person duplex	2
2-bed 3-person duplex	2
2-bed 4-person	7
<b>Total</b>	<b>25</b>

6.13 It is noted that there are no 3 bed dwellings proposed however this is considered to be reasonable for a development in this location and of this number of dwellings. Furthermore, the narrow and irregular shaped plot is a further physical constraint on the provision of larger units.

6.14 Policy HS3 of the Core Strategy requires a 35% provision of affordable housing in all schemes of 10 units or more. The Core Strategy seeks a tenure mix of 65% affordable rent, 20% social rent and 15% intermediate tenures. The emerging policy within the draft Local Plan will again seek for 35% affordable housing however with a tenure split of 60% social rent and 40% intermediate tenures.

6.15 The development is proposed with no on-site affordable housing and has been submitted with a viability assessment which has been reviewed by consultants appointed by the Council. The review undertaken by the Council's consultants has confirmed that the viability of the development would not allow for a policy compliant affordable housing provision. This finds that the scheme would be in deficit and cannot afford to provide on-site affordable housing nor any commuted sum financial payment towards off site affordable housing. This is based on a reduced level of profit at 17.5% of Gross Development Value

which is within the guideline range of 15-20% considered reasonable in the government's Planning Practice Guidance. It is however recommended that the S106 includes a review mechanism to allow for a review of the development finance towards the end of the project and allow a financial payment to be made towards affordable housing should the development have an improved financial outcome.

6.16 (e) Quality of residential accommodation

6.17 *Gross Internal Areas*

Section 7.3.6 of the Residential Design Guide sets out the minimum Gross Internal Areas for new dwellings in accordance with the Nationally Described Space Standards (NDSS). The proposal provides 1 and 2 bedroom units. All of the proposed units would meet the minimum floorspace standard for the dwelling type proposed and are compliant with the NDSS.

6.18 *Light and outlook*

The achievement of good light and outlook to dwellings of the development has the potential to be limited by the narrow width of the site and built up nature of its context. A daylight/sunlight assessment following the British Research Establishment's (BRE) best practice guidance has been undertaken assessing the Vertical Sky Component (VSC), Average Daylight Factor (ADF) and Annual Probable Sunlight Hours (APSH) for the proposed new homes in the development.

6.19 This assessment finds that all rooms of the development would have acceptable daylight in respect of the VSC calculations. Due to the layout of the development with some narrow rooms, 16 rooms of the development would have daylight distribution in the rooms below the guidelines for the ADF sought, however, these would relate to 10 bedrooms and 6 Living/Kitchen/Dining rooms where only the kitchen areas are affected, with good daylight to the living and dining areas. This would therefore not undermine or prevent reasonable living environments.

6.20 In respect of sunlight, the assessment finds that 16 rooms of the development would fall below standards for year round APSH. Of the 16 rooms affected, 5 rooms are restricted only by their own balconies meaning that occupiers would still benefit from direct sun on balconies. Of the other 11 rooms, these are all north facing and so would be expected not to have direct sunlight. It is noted that all of dwellings with 11 north facing rooms below sunlight standards would have other rooms on other orientations which would meet sunlight standards. As such, the overall environment for these dwellings would

be good despite the constraints of orientation, layout and the urban context of the site.

6.21 In general terms, it is welcomed that all dwelling are dual aspect with most dwellings having windows to both north-east and south-west elevations. It is also noted that the layout has also included only secondary or non-habitable room windows to the flank elevations to ensure that this does not unreasonably prejudice development of adjacent sites.

6.22 *Amenity areas*

All but 4 dwellings of the development would include a private amenity space of a terrace or balcony. The first floor podium level would provide a central communal amenity area of approximately 150sqm in the form of a courtyard. This area would be below the 395sqm guidance for this number of dwellings and the area would also suffer from some overshadowing, particularly in winter months. It is however considered that subject to high quality and shade tolerant landscaping, the area would provide attractive and useable amenity space for the occupiers of the 1 and 2 bedroom dwellings and would be reasonable for a development within an urban town centre setting.

6.23 *Noise, odour and Air Quality*

The submitted acoustic report demonstrates that appropriate noise environments can be achieved within the development and the recommendations of the report are secured by condition. A condition is also recommended in respect of plant and extraction required for the two commercial premises to secure details of this plant along with noise and odour assessment details.

6.24 (f) Impacts on surrounding properties

Immediately adjacent to the south east of the site is a recent development at Nos. 54A-56 High Street with residential windows on its flank wall facing onto the proposed development site. A daylight/sunlight assessment following the British Research Establishment's (BRE) best practice guidance has been undertaken assessing the impact on Vertical Sky Component (VSC), Average Daylight Factor (ADF) and Annual Probable Sunlight Hours (APSH) to the adjacent development. This assessment is based on the 'good neighbour' principles of the BRE guidance in order to fairly assess the impact of the proposed development to the existing development. This concludes that the impacts are reasonable and acceptable.

6.25 All windows on the south east elevation of the proposed development facing Nos. 54A-56 are proposed as obscurely glazed to prevent unreasonable overlooking to the adjacent residential development.

- 6.26 The omission of windows to habitable rooms on the north-west flank elevation will avoid overlooking or hindrance to the potential development of the neighbouring site to the north-west.
- 6.27 (g) Car parking, access and transportation  
The site is in a sustainable town centre location, close to amenities and public transport links. In accordance with Policy T26 of the Core Strategy, the site is within an area that would be suitable for car-free residential development and parking in surrounding roads can be prevented by the exemption of residents from parking permit entitlements. Car-free developments of this nature can make a valuable contribution towards addressing climate change.
- 6.28 (h) Trees and landscaping  
The site does not contain any trees protected by a tree preservation order or conservation area status and does not include any trees considered as significant for retention. There are no trees on adjacent land require to have protection during the construction.
- 6.29 The landscaping scheme is indicative and is to be secured by condition to ensure that the area includes landscaping for the podium amenity area to create a useable space and for ecology enhancement of the site. It is noted that there will be some shadowing within the space due to the orientation of the development however this is reasonable and should be reflected in the landscaping scheme to ensure shade tolerant planting in more shaded areas.
- 6.30 (i) Environmental impacts  
The application is accompanied by reports detailing assessments in respect of flood risk, energy and sustainability and noise impact. These technical matters have been demonstrated to be acceptable to support the planning application and relevant requirements are secured by condition.
- 6.31 (i) Section 106 and Community Infrastructure Levy (CIL)  
The Council introduced the Community Infrastructure Levy (CIL) with effect from 1 April 2015. The CIL charge covers a wide range of infrastructure as set out in the Council's Regulation 123 list, including highways and transport improvements, education provision, youth facilities, childcare facilities, children's play space, adult care services, open space and sports facilities. CIL is chargeable on the relevant net additional floorspace created by the development. The charge is non-negotiable and is calculated at the time that planning permission is granted. The CIL charge applicable to the proposed development is c.£157 per m<sup>2</sup> (including indexation).

6.32 S.106 planning obligations can only be used to secure affordable housing provision and other site specific requirements. The planning obligations for this development are set out in Section 8 of this report.

## 7. Consultation responses received

### 7.1 Statutory consultees and other organisations

Consultee	Comments	Officer response
HCC Highway Authority	Initial comments required further information. This information was provided by the applicant and has addressed the concerns of the Highway Authority. They have confirmed no objection subject to conditions recommended.	Noted
HCC Lead Local Flood Authority	Relevant and acceptable details have been provided and conditions recommended.	Noted
HCC Waste and Minerals	General comments made.	Noted
HCC Growth and Infrastructure	No response.	Noted that Community Infrastructure Levy is applicable.
Crime Prevention	A Formal Secured by Design application should be submitted by the applicants.	Noted.
Thames Water	Condition requested for piling method statement in respect of sewer pipes.	Noted and condition added.
Fire Service	No response.	Noted
Environment Agency	No response.	Noted

### 7.2 Internal Consultees

Consultee	Comments	Officer response
Contamination officer	Air quality impacts require assessment. No objection regarding contamination subject to	Noted and conditions added.

	recommended condition.	
Head of Housing	The scheme of 25 units should provide 7 or 8 affordable units. Viability report is noted however with no affordable housing offer, the development is not supported by the housing team.	Noted.
Waste and recycling officer	No response received.	Noted that internal bin storage area is included at ground floor with access onto Wellstones.
Environmental Health	No comments received.	The submitted technical assessments is sufficient to confirm appropriate noise mitigation and this is conditioned.
Urban Design and Conservation Manager	Ongoing input into the design development.	Case officer has been working collaboratively with Urban Design and Conservation Manager through pre-application process to positively inform the development.

### 7.3 Interested parties

The application was advertised by letters to neighbours, site notices and paper adverts. The following is a summary of the representations that have been received:

Number of original consultations:	46
Number of objections:	4
Number in support:	0
Number of representations:	0
<b>TOTAL REPRESENTATIONS:</b>	<b>4</b>

Comments	Officer response
8 storey height is too tall	The 8 storeys is within what is considered as a 'base height' for development in this location as set out in the draft Local Plan. It is also in keeping with the 7 storeys at the adjacent site and 8 storeys approved at the rear of No. 60 High Street.
No car parking	The site is within a highly sustainable location suitable for people to live without cars. Residents would not be entitled to park in surrounding roads.
Loss of the existing buildings is loss to history of the town and the facades should be kept.	The buildings are not of an architectural or historic value that warrants retention as statutorily listed buildings. As local buildings of interest, the design quality of the new building onto the High Street does justify the loss of the existing buildings and allow for sustainable development of new homes.
The proposed buildings have lack of creativity and architectural merit.	The design is successful in addressing the constraints of the site and the design quality is considered to be appropriate for the position and height of the development.
The development would demolish two business premises which is harmful to the town centre.	The development re-provides two new commercial units to enhance the town centre offer.

## 8. Recommendation

- 8.1 That, pursuant to a planning obligation under s.106 of the Town and Country Planning Act 1990 having been completed to secure the following Heads of Terms, planning permission be granted subject to the conditions listed below:

### Section 106 Heads of Terms

- i) To secure a financial payment to the Council of £2,000 towards the variation of the Borough of Watford (Watford Central Area and West Watford Area) (Controlled Parking Zones) (Consolidation) Order 2010 to exclude the site from the controlled parking zone, thereby preventing residents' parking permits being issued to the future occupiers of this site.

- ii) To secure a review mechanism of the viability of the development to be undertaken towards the end of the project (at 75% of flat sales) when actual build costs and sales values of the flats are known. This shall allow a financial payment to be made towards affordable housing provision where the viability of the development can be shown to have improved to provide a financial surplus.

## Conditions

### 1. Time Limit

The development to which this permission relates shall be begun within a period of three years commencing on the date of this permission.

Reason: To comply with the requirements of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

### 2. Approved Drawings and Documents

The development hereby permitted shall be carried out in accordance with the following approved drawings and documents:

PL001 Site Location Plan  
PL100/A Existing Ground Floor Plan  
PL101/A Existing First Floor Plan  
PL102/A Existing Second Floor Plan  
PL103/A Existing Roof Plan  
PL200/G Proposed Ground Floor Plan  
PL201/F Proposed First Floor Plan  
PL202/E Proposed Second Floor Plan  
PL203/E Proposed Third Floor Plan  
PL204/E Proposed Fourth Floor Plan  
PL205/E Proposed Fifth Floor Plan  
PL206/E Proposed Sixth Floor Plan  
PL207/E Proposed Seventh Floor Plan  
PL208/D Proposed Roof Plan  
PL400/C Proposed North and South Elevations- North block  
PL401/F Proposed North and South Elevations- South block  
PL402/E Proposed South-East Elevation  
PL403/E Proposed North-West Elevation  
PL510/C Proposed Longitudinal section  
20046-01-006B Proposed Footway  
20046-01-003B Articulated Vehicle tracking  
Design and Access Statement reference A2156-PL-DAS Rev B

Reason: For the avoidance of doubt and in the interests of proper planning.

### 3. Surface Water Assessment

The development permitted by this planning permission shall be carried out in accordance with the approved surface water drainage assessment carried out by Flo Consult UK Ltd dated January 2021, project number 570 Revision D and the following mitigation measures detailed within the FRA:

1. Limiting the surface water run-off generated by the critical storm events so that it will not exceed the surface water run-off rate of 2.7 l/s during the 1 in 100 year event plus 40% of climate change event.
2. Providing storage to ensure no increase in surface water run-off volumes for all rainfall events up to and including the 1 in 100 year + climate change event providing a minimum of 53m<sup>3</sup> (or such storage volume agreed with the LLFA) of total storage volume in blue roof and underground attenuation tank.
3. Discharge of surface water from the private network to the Thames Water surface water sewer.

The mitigation measures shall be fully implemented prior to occupation and subsequently in accordance with the timing / phasing arrangements embodied within the scheme, or within any other period as may subsequently be agreed, in writing, by the local planning authority.

Reason: To prevent flooding by ensuring the satisfactory disposal and storage of surface water from the site and to reduce the risk of flooding to the proposed development and future occupants.

### 4. Surface Water Drainage Scheme

No development shall take place until a detailed surface water drainage scheme for the site based on the approved drainage strategy and sustainable drainage principles, has been submitted to and approved in writing by the Local Planning Authority. The drainage strategy should demonstrate the surface water run-off generated up to and including 1 in 100 year + climate change critical storm will not exceed the run-off from the undeveloped site following the corresponding rainfall event. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed.

1. Detailed engineered drawings of the proposed SuDS features including cross section drawings, their size, volume, depth and any inlet and outlet features including any connecting pipe runs.

2. Final detailed post-development drainage calculations for all rainfall events up to and including the 1 in 100 + 40% climate change event including half drain down times.
3. Exceedance flow paths for surface water for events greater than the 1 in 100 year including climate change allowance.
4. Final detailed management plan to include arrangements for adoption and any other arrangements to secure the operation of the scheme throughout its lifetime.

Reason: To prevent the increased risk of flooding, both on and off site.

5. New Access Surface Water

Prior to the first occupation of the development hereby permitted the vehicular access shall be completed and thereafter retained as shown on drawing number 20046-01-006 Rev B in accordance with details/specifications to be submitted to and approved in writing by the Local Planning Authority in consultation with the Highway Authority. Prior to use appropriate arrangements shall be made for surface water to be intercepted and disposed of separately so that it does not discharge from or onto the highway carriageway.

Reason: To ensure satisfactory access into the site and avoid carriage of extraneous material or surface water from or onto the highway in accordance with Policy 5 of Hertfordshire's Local Transport Plan (adopted 2018).

6. Highway Improvements - pavement

Notwithstanding the details indicated on the submitted drawings, no on-site works above slab level shall commence until a detailed scheme for the off-site highway improvement works as indicated on drawing number 20046-01-006 Rev B have been submitted to and approved in writing by the Local Planning Authority.

Prior to the first use of the development hereby permitted, the improvement works referred to in this condition shall be completed in accordance with the approved details.

Reason: To ensure construction of a satisfactory development and that the highway improvement works are designed to an appropriate standard in the interest of highway safety and amenity and in accordance with Policy 5, 13 and 21 of Hertfordshire's Local Transport Plan (adopted 2018).

7. Construction Management Plan

No development shall commence until a Construction Management Plan has been submitted to and approved in writing by the Local Planning Authority.

Thereafter the construction of the development shall only be carried out in accordance with the approved Plan. The Construction Management Plan shall include details of:

- a. Construction vehicle numbers, type, routing;
- b. Access arrangements to the site;
- c. Traffic management requirements;
- d. Construction and storage compounds (including areas designated for car parking, loading / unloading and turning areas);
- e. Siting and details of wheel washing facilities;
- f. Cleaning of site entrances, site tracks and the adjacent public highway;
- g. Timing of construction activities (including delivery times and removal of waste) and to avoid school pick up/drop off times;
- h. Provision of sufficient on-site parking prior to commencement of construction activities;
- i. Post construction restoration/reinstatement of the working areas and temporary access to the public highway;
- j. Where works cannot be contained wholly within the site a plan should be submitted showing the site layout on the highway including extent of hoarding, pedestrian routes and remaining road width for vehicle movements.

Reason: In order to protect highway safety and the amenity of other users of the public highway and rights of way in accordance with Policies 5, 12, 17 and 22 of Hertfordshire's Local Transport Plan (adopted 2018).

#### 8. Piling Method Statement

No piling shall take place until a Piling Method Statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to and approved in writing by the Local Planning Authority. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

Reason: The proposed works will be in close proximity to underground sewerage utility infrastructure. Piling has the potential to significantly impact / cause failure of local underground sewerage utility infrastructure.

#### 9. Reporting of Unexpected Contamination

In the event that contamination is found at any time when carrying out the approved development that was not previously identified it must be reported in writing immediately to the Local Planning Authority. An investigation and

risk assessment must be undertaken, and where remediation is necessary a remediation scheme must be prepared, which is subject to the approval in writing of the Local Planning Authority. Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

#### 10. Materials

No external facing materials shall be installed on the building until:

- a. full details of the materials to be used for all the external surfaces have been submitted to and approved in writing by the Local Planning Authority; and
- b. sample panels have been constructed on site to show the typical facades including brickwork, window frames and glazing, made available for inspection and approved in writing by the Local Planning Authority.

Reason: In the interests of the visual appearance of the building and the character and appearance of the area, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

#### 11. Hard landscaping

No part of the development shall be occupied until full details of a hard landscaping scheme have been submitted to and approved in writing by the Local Planning Authority, and the works have been carried out in accordance with the approved details. The detailed scheme shall be based upon the Landscape Strategy of the Design and Access Statement reference A2156-PL-DAS Rev B and shall include details of seating, planters, site boundary treatments, lighting and the materials and drainage of all hardstanding.

Reason: In the interests of the visual appearance of the site and the local area pursuant to Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

#### 12. Soft Landscaping

No part of the development shall be occupied until a detailed soft landscaping scheme for the site and a landscape management and maintenance plan, has been submitted to and approved in writing by the Local Planning Authority.

The detailed scheme shall be based upon Landscape Strategy of the Design and Access Statement reference A2156-PL-DAS Rev B and shall include details of planting appropriate for the sunlight conditions for the relevant areas. The approved soft landscaping scheme shall be carried out not later than the first available planting and seeding season after completion of development. Any trees or plants whether new or existing which within a period of five years die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species, or in accordance with details approved by the Local Planning Authority.

Reason: In the interests of the visual appearance of the site and the wider area and to ensure amenity and ecology enhancements, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

### 13. Plant

No part of the commercial premises of the development shall be occupied until details of all plant, ventilation and extraction systems for the commercial premises have been submitted to and approved in writing by the Local Planning Authority and the works have been carried out in accordance with the approved details. Details shall include appropriate noise and odour/air quality assessments carried out in accordance with BS4142 and undertaken by appropriately qualified technical consultants. The approved works shall be retained at all times.

Reason: To prevent noise and odour disturbance and in the interests of amenities of neighbouring and future occupiers of the development.

### 14. Noise mitigation

No part of the development shall be occupied until the sound insulation measures have been installed in line with the specifications and recommendations of the Acoustic Design Statement Technical Report, ref 34109-R2, dated 12/01/2021 prepared by Sound Solutions Consultants. These measures shall be retained at all times.

Reason: In the interests of amenities of future occupiers of the development.

### 15. Refuse and recycling

No part of the development shall be occupied until the refuse and recycling facilities shown in the Design and Access Statement and on drawing number PL200 Rev G have been constructed and made available for use. These facilities shall be retained as approved at all times.

Reason: In the interests of the visual appearance of the site and to ensure that adequate facilities exist for residents of the proposed development, in accordance with saved Policy SE7 of the Watford District Plan 2000.

16. Cycle storage

No part of the development shall be occupied until the cycle parking facilities shown in the Design and Access statement and on drawing number PL200 Rev G have been constructed and made available for use. These facilities shall be retained as approved at all times.

Reason: To encourage travel by cycle and to provide sustainable travel alternatives, in accordance with saved Policy T10 of the Watford District Plan 2000 and Policy T3 of the Watford Local Plan Core Strategy 2006- 31.

17. Communal Satellites/aerials

No dwelling shall be occupied until details of communal terrestrial television aerial(s) and satellite dish(es) to serve the dwellings have been submitted to and approved in writing by the Local Planning Authority and the works have been carried out in accordance with the approved details. No other aerials or satellite dishes shall be installed on the buildings.

Reason: In the interests of the character and appearance of the building, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

18. Retention of lobby and glazing

No part of the development shall be occupied until the residential entrance lobby from Wellstones has been installed as shown in the approved drawings and sections 10.0 and 11.0 of the Design and Access Statement. The entrance lobby layout, facilities and glazing shall be retained as approved at all times.

Reason: In the interests of amenities of occupiers and in the interests of the character and appearance of the building, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

19. Obscure glazing

The upper floor windows in the south east side elevation shown in the approved drawings to be obscurely glazed, shall be installed and retained with obscure-glazing, and shall be non-opening other than in parts of the windows which are more than 1.7 metres above the floor of the room in which the window is installed.

Reason: To prevent overlooking and consequent loss of privacy to neighbouring premises pursuant to Policies SS1 and UD1 of the Watford Local Plan Core Strategy 2006-31 and the Residential Design Guide 2016.

#### Informatives

1. IN907 – Positive and proactive statement
2. IN909 – Street naming and numbering
3. IN910 – Building Regulations
4. IN911 – Party Wall Act
5. IN912 – Hours of Construction
6. IN913 – Community Infrastructure Levy Liability
7. IN915 – Highway Works – HCC agreement required

# Site Location Plan



01: Block Plan

Rev.	Date	Description
1	20.03.19	Issued for Planning

Notes

Key
<span style="color: red;">—</span> Application Site
<span style="color: blue;">—</span> Extent of Ownership

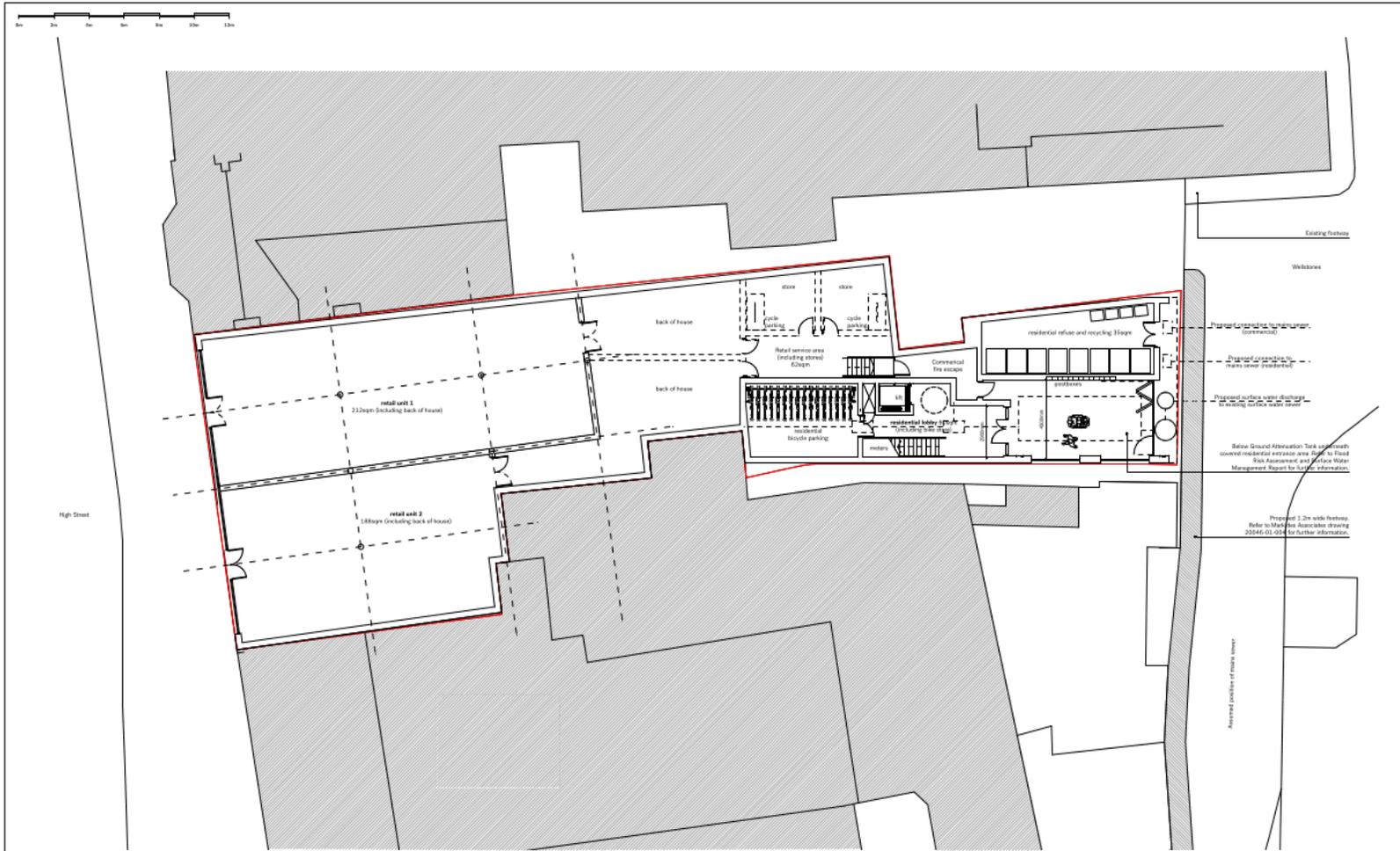


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 E: info@nissens.com  
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Project	46 and 46-50 High Street, Watford WD17 2JG
Client	46 and 46-50 High Street Limited
Title	Block Plan
Scale	1:250 at A1 1:500 at A2
Drawing no.	<b>A2156-PL-010</b>

Nissen Richards Group Limited registered in England & Wales no. 87128133

# Proposed Ground Floor



01: Proposed Ground Floor Plan (Level 0)

Rev	Date	Description
1	20/01/24	Issued for Planning
2	20/01/24	Corrected to include sewer outlet
3	20/05/24	Amended layout following comments
4	02/06/24	Amended layout following BMR
5	12/06/24	Submitted for the application
6	02/12/24	Submitted for Planning Application
7	09/12/24	Planning application amended
8	09/04/25	Final ground level and commercial cycle parking added

Notes

Plan

Site Boundary

**Nissen Richards**

Unit 5, 6, Waterhouse  
 3, Crown Road  
 London, E1 1JZ

T: 020 7570 9808  
 info@nissensrichards.com  
 www.nissenrichards.com

Project: 45 and 49-53 High Street, Welford WD17 3PS

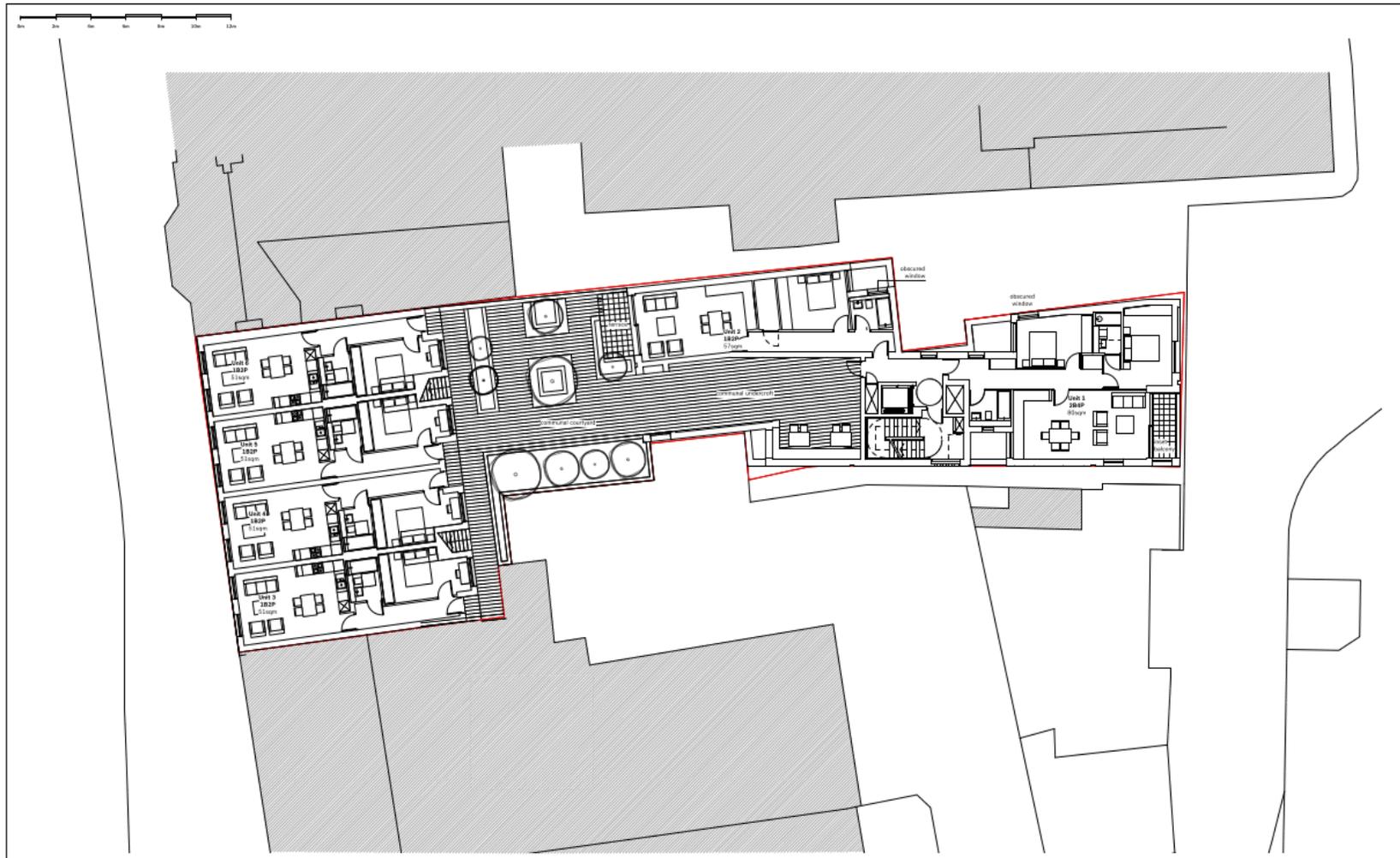
Client: 45 and 49-53 High Street Limited

Site: Proposed Ground Floor Plan Level 0

Scale: 1:100 at A1 1:200 at A3

Drawing No: **A2156-PL-200/G**

# Proposed First Floor



01: Proposed First Floor Plan (Level 1)

Rev	Date	Description
1	2022.09	Issued for Planning
A	23.03.24	Landscaping Details
B	20.06.24	Amended Planning Submission
C	02.09.24	Amended Planning Submission
D	12.10.24	Submitted for Planning
E	02.12.24	Submitted for the application
F	25.01.25	Amendments to sections

Notes

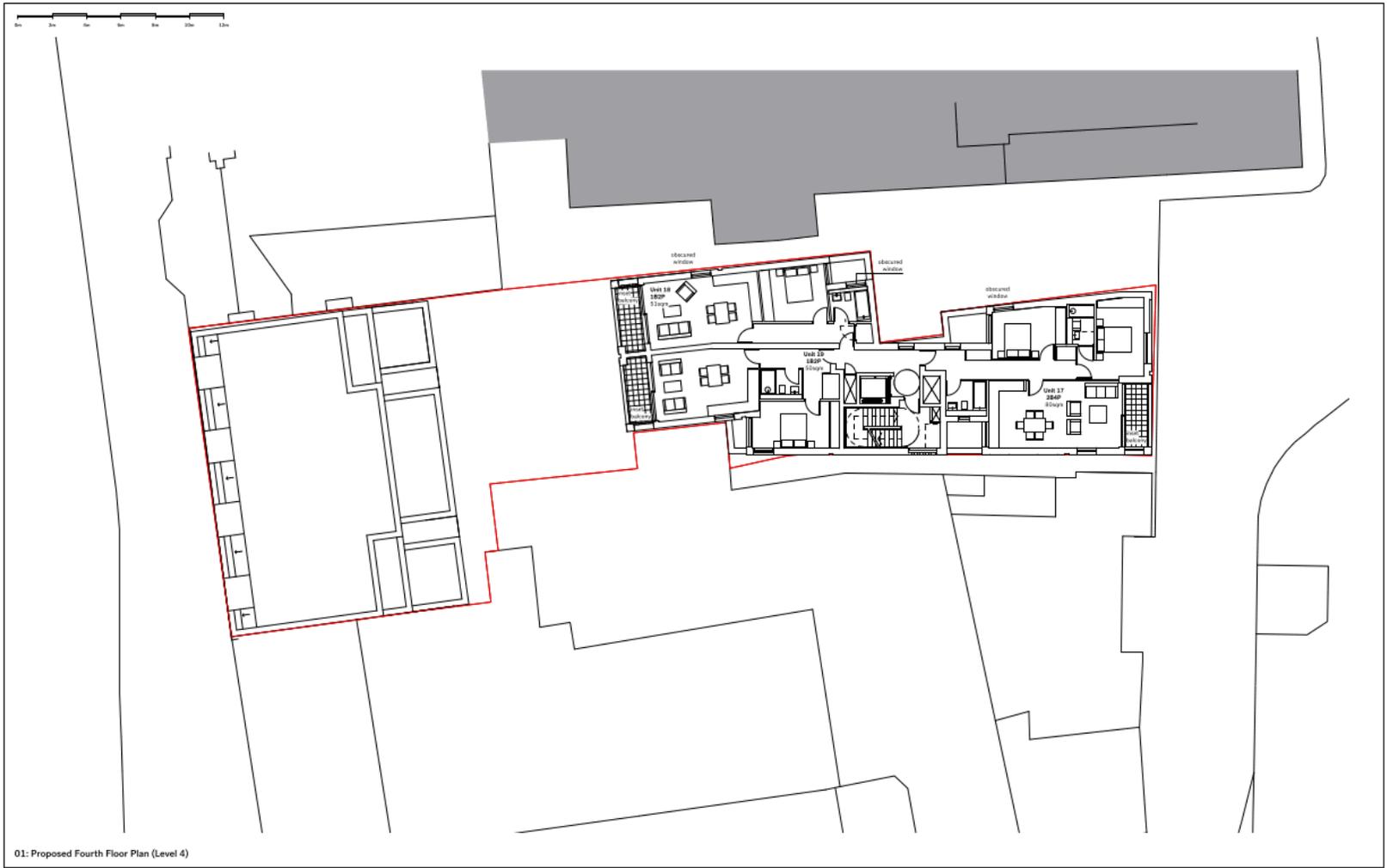


**Nissen Richards**  
 Unit 2-4 Nissen House  
 8 Clarendon Road  
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 T: 020 7310 5895  
 info@nissensrichards.co.uk  
 www.nissenrichards.co.uk

Project: 46 and 48-50 High Street, Waltham Cross 2025  
 Client: 46 and 48-50 High Street Limited  
 Title: Proposed First Floor Plan Level 1  
 Scale: 1:100 or A1 1:500 or A3  
 Drawing no: **A2156-PL-201/F**

Nissen Richards Studio Limited registered in England & Wales no. 87108133

# Fourth Floor Plan



01: Proposed Fourth Floor Plan (Level 4)

Rev	Date	Description
1	20.03.18	Issued for Planning
2	03.05.18	Amended to include following comments
3	02.05.18	Amended to include following comments
4	02.05.18	Amended to include following comments
5	02.05.18	Agreed with the Applicant
7	05.01.18	Amended to include

Notes

Key
 Site Boundary



**Nissen Richards**

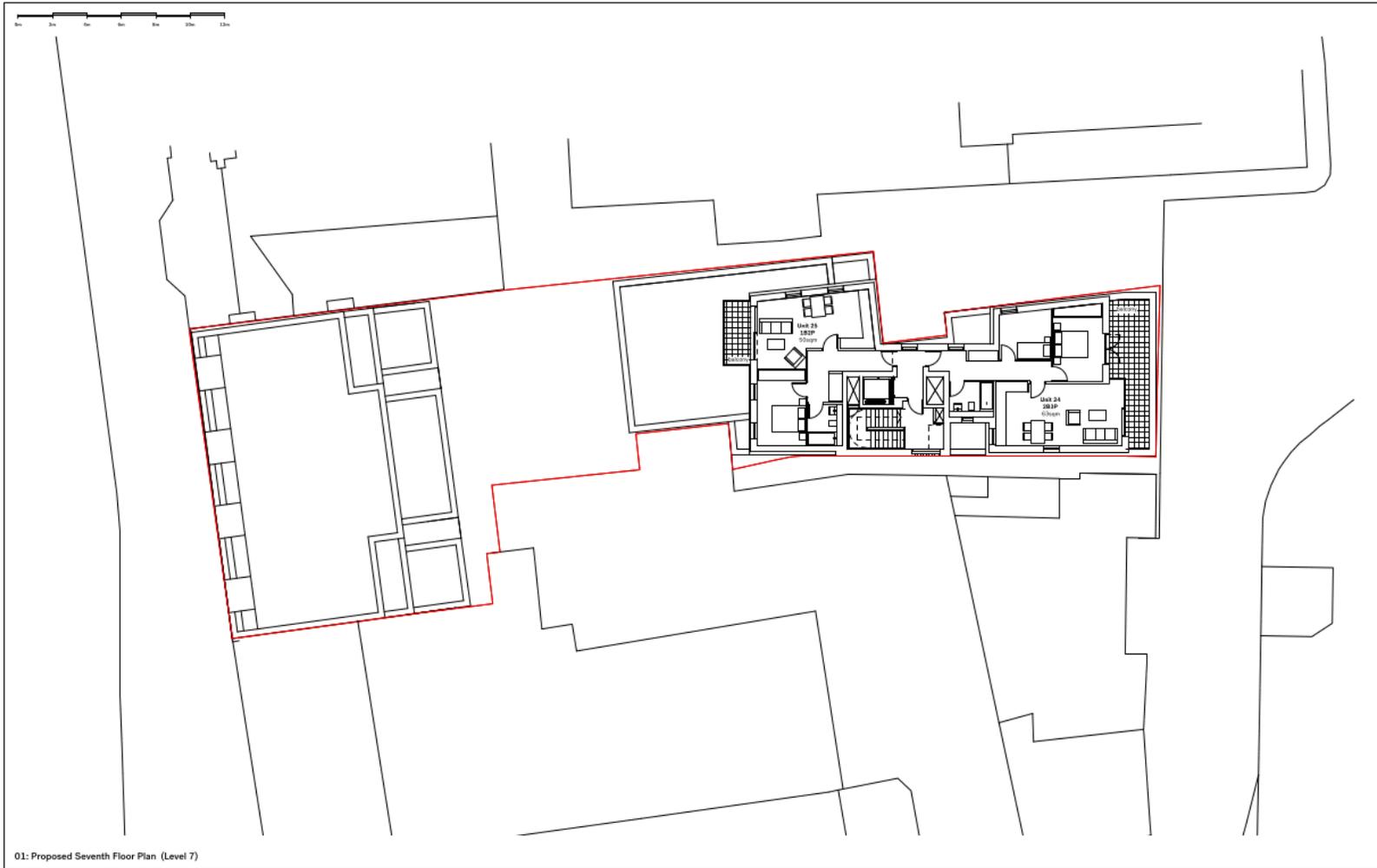
Unit 1-6, Waterhouse  
 3 Clarendon Road  
 London W11 3JL

T: 020 7333 8868  
 info@nissens.com  
 www.nissenrichards.com

Project	46 and 48-50 High Street, Watford WD17 2SS
Client	46 and 48-50 High Street Limited
Title	Proposed Fourth Floor Plan Level 4
Scale	1:100 at A1 1:200 at A2
Drawing No.	<b>A2156-PL-204/E</b>

Nissen Richards (Global) Limited registered in England & Wales no. 87128132

# Seventh Floor Plan



01: Proposed Seventh Floor Plan (Level 7)

Rev	Date	Description	Notes
1	20.03.19	Issued for Planning	
2	20.03.19	Amended layout following comments	
3	02.04.19	Amended layout following comments	
4	12.05.19	Amended layout following comments	
5	02.12.19	Submitted for the application	
6	05.01.20	Amendments to windows	

Key

— Site Boundary

**Nissen Richards**

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 2, Colmore Row  
 London EC1A 3JF

T 020 7573 0800  
 info@nissens.com  
 www.nissen-richards.co.uk

Project: 45 and 48-50 High Street, Watford WD17 2BS

Client: 45 and 48-50 High Street Limited

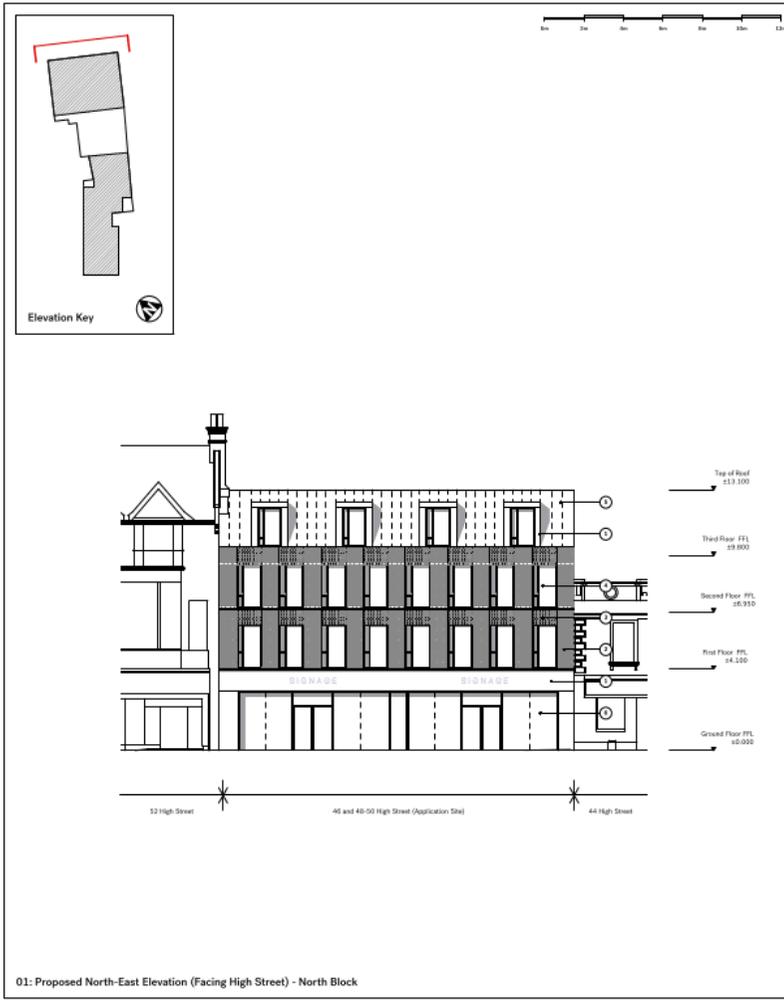
Title: Proposed Seventh Floor Plan Level 7

Scale: 1:100 at A1 1:500 at A3

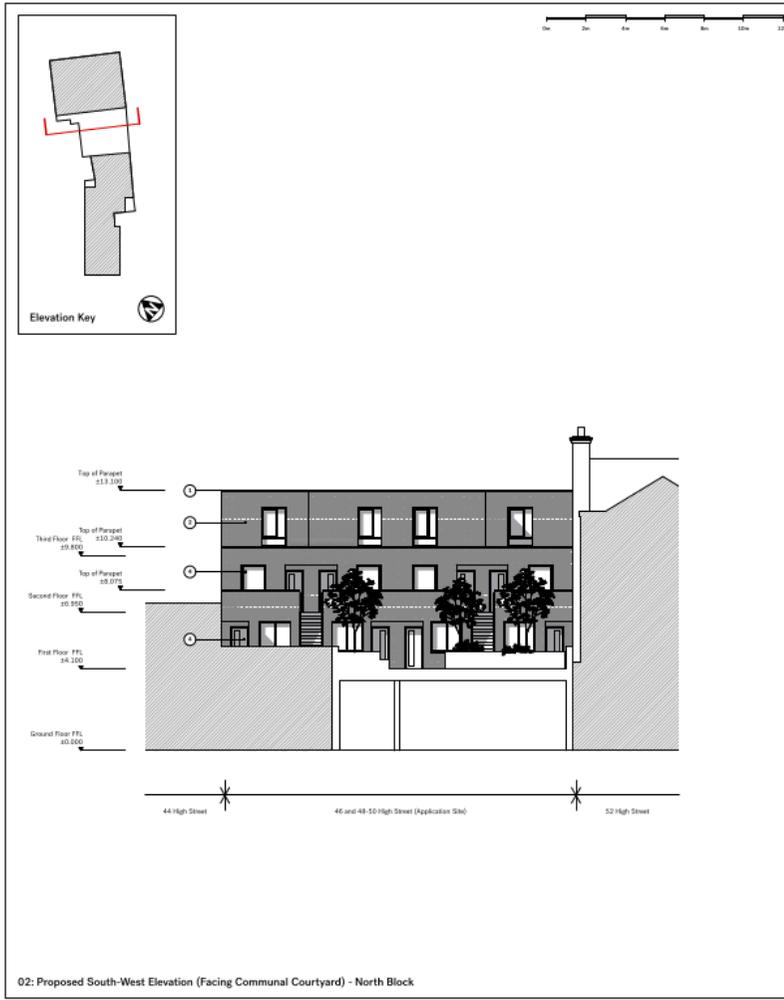
Drawing no: **A2156-PL-207/E**

Nissen Richards Global Limited registered in England & Wales no. 9728112

# North Block elevations



01: Proposed North-East Elevation (Facing High Street) - North Block



02: Proposed South-West Elevation (Facing Communal Courtyard) - North Block

Rev	Date	Description
A	20.03.20	Issued for Planning
B	27.03.20	Final to floor heights amended
C	02.09.20	Amendments following EIR
C	03.12.20	Amendments for P10 application

Name:

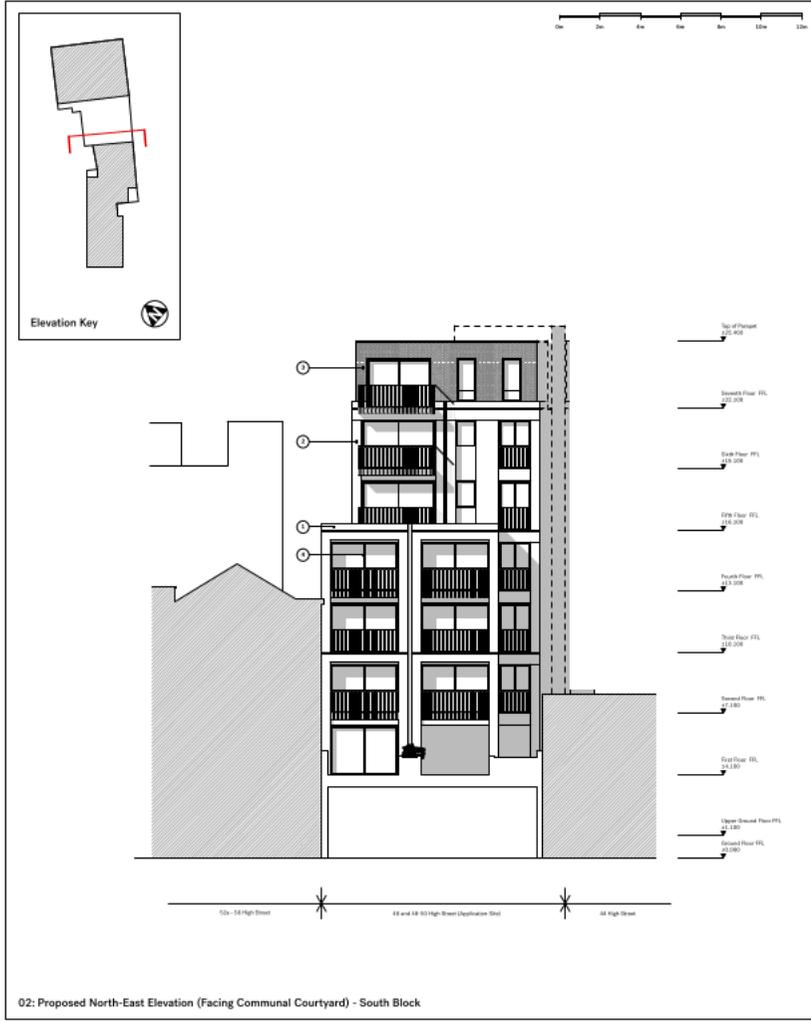
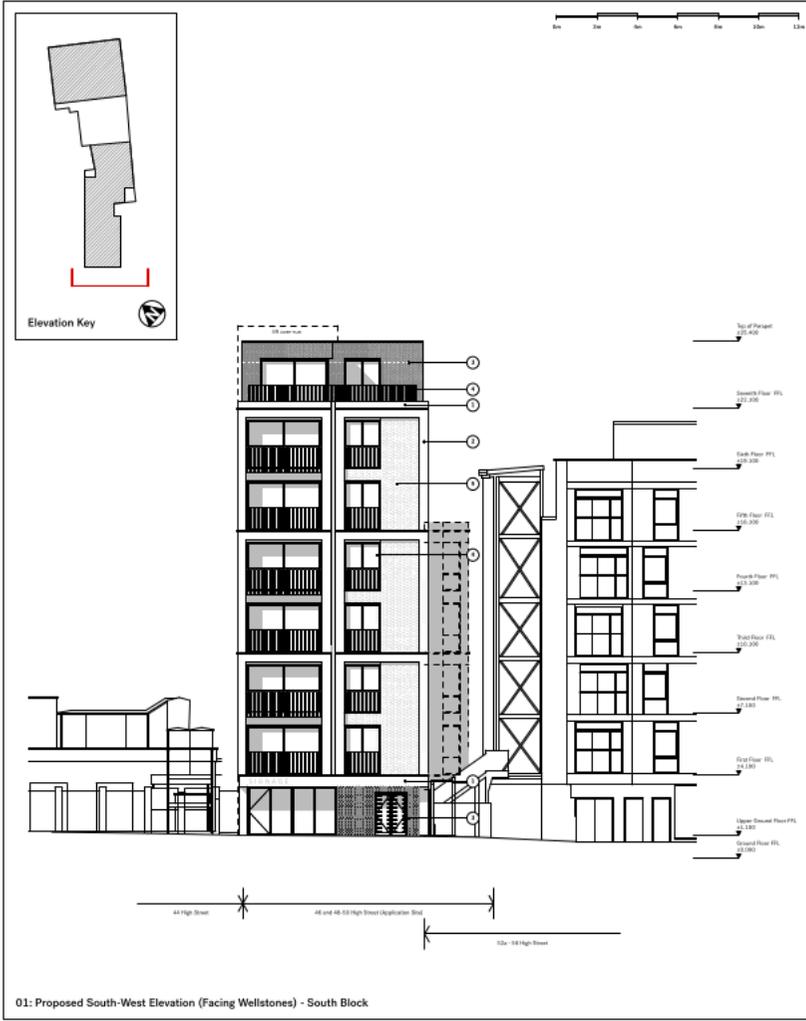
- Material Key:
- ① Pre-cast concrete
  - ② Stricken bond brickwork
  - ③ Sawtooth brickwork
  - ④ Anodised/Powder Coated aluminium window and door frames with clear glazing
  - ⑤ Anodised Metal roofing with vertical slanting eaves
  - ⑥ Curtain wall glazing to retail frontage

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Project: 46 and 48-50 High Street, Watford WD17 2BS  
Client: 46 and 48-50 High Street Limited  
Site: Proposed North and South Elevations North Block  
Scale: 1:100 at A1 1:500 at A3  
Drawing no: **A2156-PL-400/C**

# South Block Elevations



Rev	Date	Description	Notes
1	20.02.18	Issued for Planning	
2	27.02.18	Minor Amendments	
3	02.04.18	Amendments following DMR	
4	22.04.18	Amendments following comments	
5	02.12.18	Submitted for Planning	
6	02.12.18	Planning Officer Approved	
7	10.01.21	Amendments to Top Floor	

- Materials:**
- ① pre-cast concrete
  - ② stretcher bond brickwork
  - ③ sandrally brickwork
  - ④ modular aluminium windows, doors and balconies
  - ⑤ Flemish bond brickwork with semi-recessed headers

**Nissen Richards**

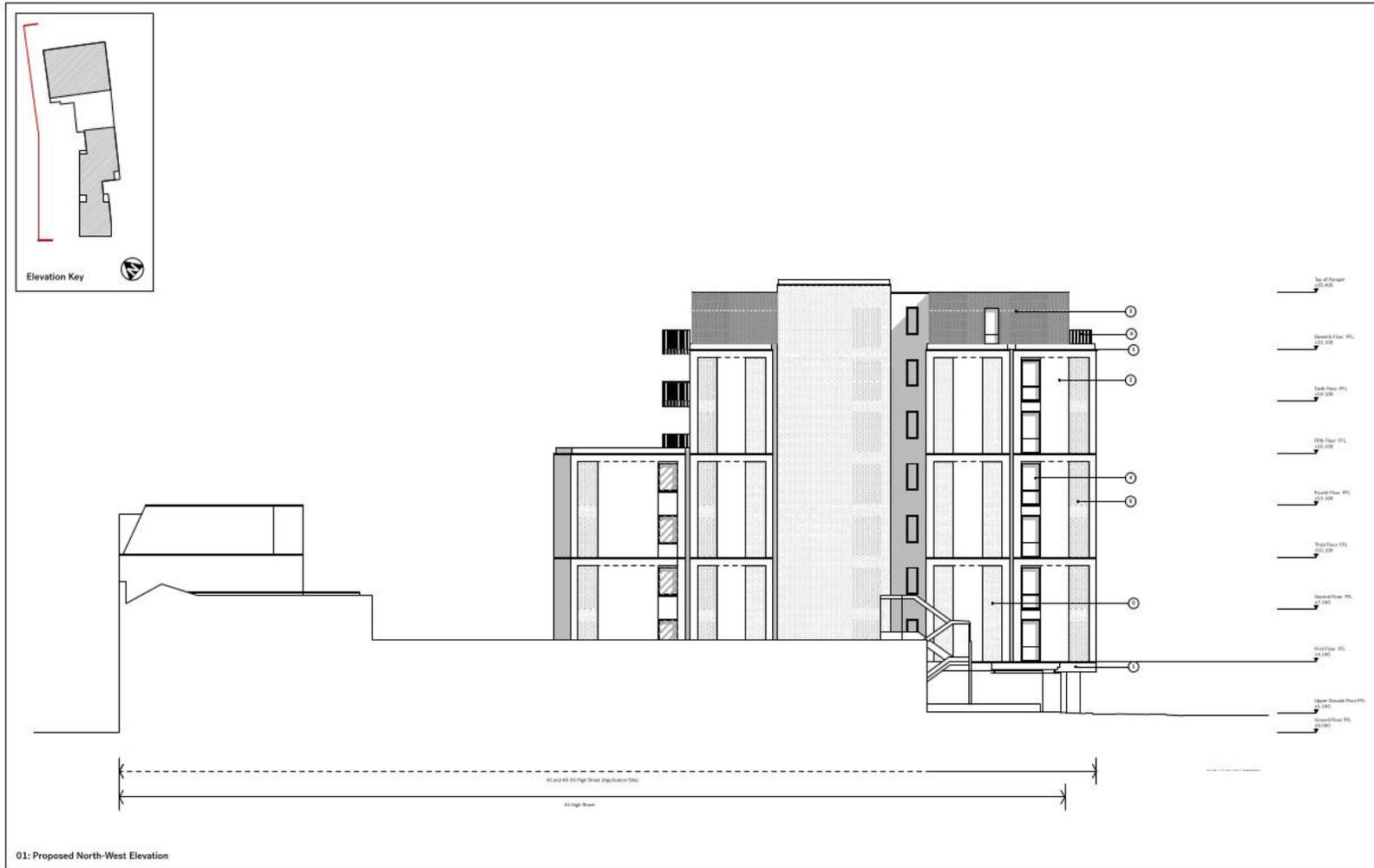
1 and 2, 6 Waterhouse  
 10 Darnley Road  
 London W11 1QZ

T: 020 7433 6808  
 nrichards@nissenrichards.com  
 www.nissenrichards.com

**Project:** 45 and 48-50 High Street, Welford WD17 2SS  
**Client:** 45 and 48-50 High Street Limited  
**Title:** Proposed North and South Elevations South Block  
**Scale:** 1:100 w/ A1, 1:200 w/ A2  
**Drawing No:** A2156-PL-401/F

Nissen Richards Studio Limited registered in England & Wales no. 871206113

# North-West Side Elevation



01: Proposed North-West Elevation

Rev	Date	Description
1	20.03.19	Issued for Planning
2	27.03.19	Minor Amendment
3	05.04.19	Approved by Planning Dept
4	12.05.19	Submitted for the Application
5	02.12.19	Submitted for the Application
6	25.01.21	Amendments to First Floor

Notes

Key	Description
1	Precast concrete
2	Stretch bond brickwork
3	Sawtooth brickwork
4	Anodised aluminium windows, doors and subframes
5	Flintish Band brickwork with zinc-plated hardware
6	TRE and M&C brickwork

**Nissen Richards**

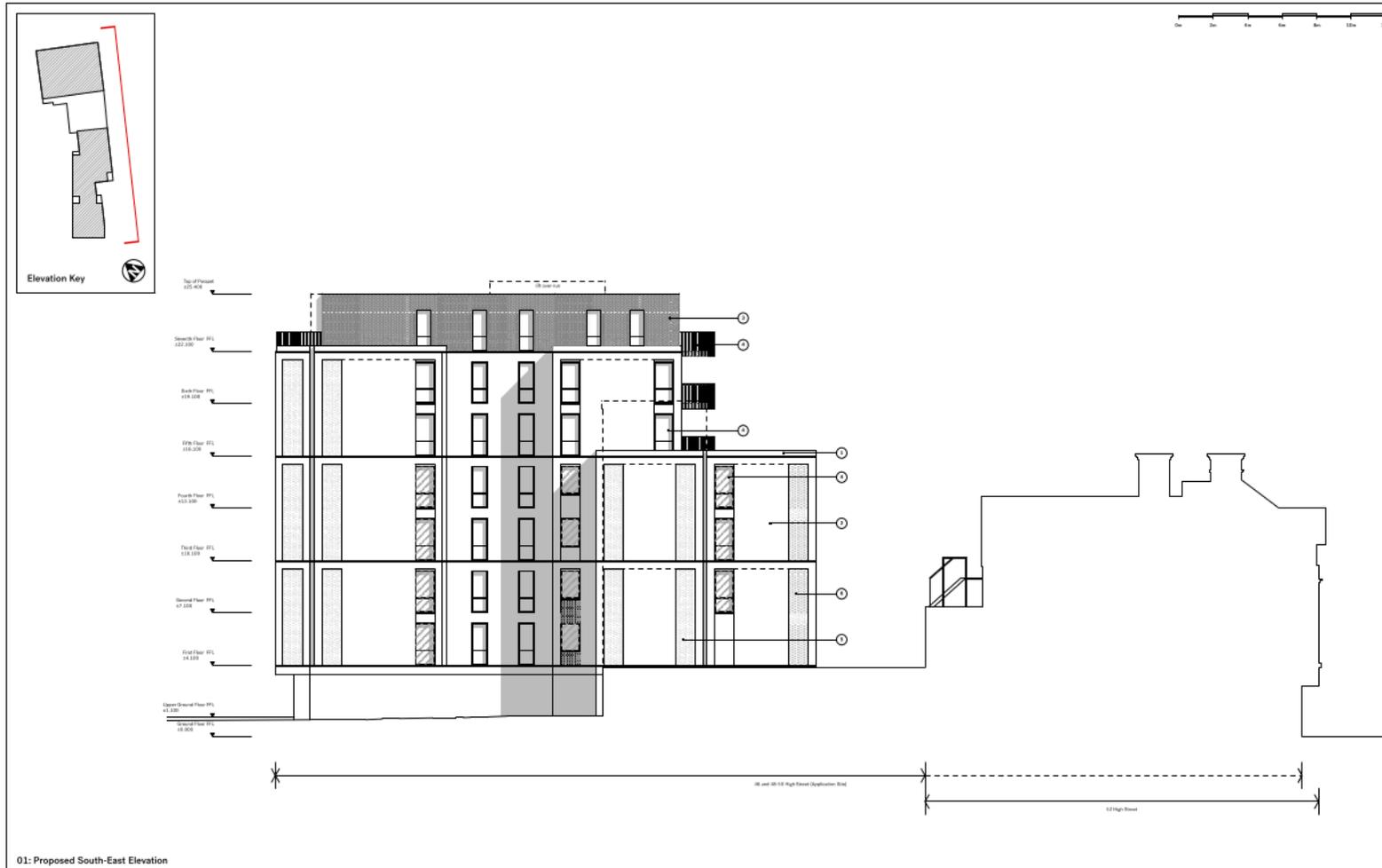
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 70 Darnley Road  
 Levenshulme, Manchester M13 2JZ

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 nrichards@nissensrichards.com  
 www.nissenrichards.co.uk

Project	45 and 45-50 High Street, Westford WDL7 2BS
Client	45 and 45-50 High Street Limited
Title	Proposed North-West Elevation
Scale	1:100 at A1 1:200 at A3
Drawing No.	A2156-PL-403/E

Nissen Richards Studio Limited registered in England & Wales No. 87138113

# South-east side elevation



01: Proposed South-East Elevation

<table border="1"> <thead> <tr> <th>Rev</th> <th>Date</th> <th>Description</th> </tr> </thead> <tbody> <tr> <td>A</td> <td>20.03.18</td> <td>Issued for Planning</td> </tr> <tr> <td>B</td> <td>27.03.18</td> <td>Minor Amendments</td> </tr> <tr> <td>C</td> <td>02.05.18</td> <td>Architectural Working Draw</td> </tr> <tr> <td>D</td> <td>22.05.18</td> <td>Submitted for Planning</td> </tr> <tr> <td>E</td> <td>02.06.18</td> <td>Submitted for Planning</td> </tr> <tr> <td>F</td> <td>25.06.18</td> <td>Amendments to Top Floor</td> </tr> </tbody> </table>	Rev	Date	Description	A	20.03.18	Issued for Planning	B	27.03.18	Minor Amendments	C	02.05.18	Architectural Working Draw	D	22.05.18	Submitted for Planning	E	02.06.18	Submitted for Planning	F	25.06.18	Amendments to Top Floor	<p>Notes:</p>	<p>Key:</p> <ul style="list-style-type: none"> <li>① Pre-cast concrete</li> <li>② Stretcher bond brickwork</li> <li>③ Sawtooth brickwork</li> <li>④ Anodised aluminium windows, doors and balconies.</li> <li>⑤ Flemish Bond brickwork with arch-recessed headers</li> <li>⑥ T18 and Mar brickwork</li> </ul>	<p><b>Nissen Richards</b></p> <p>Unit 2-4, Water Towers 8, Clarence Road London SE1 3SG</p> <p>T 020 7332 8898 info@nissensrichards.co.uk www.nissenrichards.co.uk</p> <p>Project: 48 and 49-50 High Street, Westford WC22 2JF Client: 48 and 49-50 High Street Limited Title: Proposed South-East Elevation Scale: 1:300 on A1, 1:300 on A3 Drawing No: <b>A2156-PL-402/E</b></p>
Rev	Date	Description																						
A	20.03.18	Issued for Planning																						
B	27.03.18	Minor Amendments																						
C	02.05.18	Architectural Working Draw																						
D	22.05.18	Submitted for Planning																						
E	02.06.18	Submitted for Planning																						
F	25.06.18	Amendments to Top Floor																						

Nissen Richards Studio Limited registered in England & Wales no. 87338133

High Street CGI



Rear CGI





**Watford Place Shaping Review Panel**

**Report of Small Sites Review Meeting: 46 – 50 High Street**

Tuesday 18 August 2020  
via video conference

**Panel**

Peter Bishop (chair)  
Irfan Alam  
Anna Bardos

**Attendees**

Paul Baxter	Watford Borough Council
Sian Finney-MacDonald	Watford Borough Council
Ben Martin	Watford Borough Council
Alice Reade	Watford Borough Council
Adrian Harvey	Frame Projects
Penny Nakan	Frame Projects

**Observers**

Sharon Hayes	Watford Borough Council
Sean Peschiera	Watford Borough Council
Helen Harris	Watford Borough Council

**Apologies / report copied to**

Tom Bolton	Frame Projects
Louise Holman	Watford Borough Council

**Confidentiality**

This is a pre-application review, and therefore confidential. As a public organisation Watford Borough Council is subject to the Freedom of Information Act (FOI), and in the case of an FOI request may be obliged to release project information submitted for review.

# CONFIDENTIAL

## 1. Project name and site address

46 and 48-50 High Street, Watford, WD17 2BS

## 2. Presenting team

Jim Richards	Nissen Richards Studio
Andrea Hickey	Nissen Richards Studio
David Brown	Newsteer
Jessica Wilson	Newsteer
Daniel Berko	46 and 48-50 High Street Watford 2017 Limited
Laurence Beck	46 and 48-50 High Street Watford 2017 Limited

## 3. Planning authority briefing

The site is located within Watford town centre. The frontage to the High Street consists of two locally listed buildings, which form part of a wider group of listed and locally listed buildings that contribute significant group value to the character of this part of the High Street. The site backs onto Wellstones to the rear, which is a public highway but operates principally as a rear service road to High Street properties.

Adjoining the site to the south-east is a recently completed mixed-use development including two commercial units fronting the High Street and 56 flats (Nos. 52A-56, High Street). The scale of the building is four storeys on the High Street and seven storeys on Wellstones. A similar scale of development has also recently been approved at No. 60, High Street adjoining Nos. 52A-56. To the west of Wellstones is the Telephone Exchange site, which consists of a large exchange building and extensive surface-level car parking.

Officers would particularly welcome the panel's comments on the impact of the proposal on the adjoining heritage assets, as well as its assessment of whether the quality of the replacement building justifies the loss of the locally listed buildings on the site. Further, its views are sought on the scale, footprint, height and elevational treatment of the 11-storey element to the rear, as well as the quality of residential accommodation and the impact of the scheme on its neighbours.



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## 4. Panel's views

### *Summary*

The panel considers that, while the proposals have the potential to enhance the High Street frontage, significant changes are needed if the element facing onto Wellstones is to be acceptable. While it considers that the quality of the proposed design for the building fronting onto the High Street could justify the removal of the existing, locally listed buildings, subject to design refinement and quality of detailing, the design approach requires further justification through a context study. In contrast, the rear element of the building is too tall for its setting, setting a concerning precedent for future development in this part of Watford and undermining the emerging frontage on Wellstones. A considerable amount of work is required if the scheme is to provide an effective and appropriate response to its setting. The panel believes this and other problems stem from an excessive volume of development required for the site. In particular, the long blank façades to the side of the block form an awkward presence, neither a tower nor part of a sequence of buildings. The panel raises concerns over the quality of the residential accommodation proposed, relating to the internal arrangement of circulation space, the layout and lack of daylight in individual units, and the quality of the entrance. The next iteration of the scheme should also be accompanied by visualisations showing how the building sits within its context, as well as thorough daylight analysis and full data on the scheme's environmental performance. These comments are expanded below.

### *Response to context*

- The panel recognises the difficulties presented by the site, but does not consider these to be unusual in a town centre site. Despite these challenges, the scheme occupies a sensitive and prominent position and therefore needs to provide high quality development that will enhance both the High Street and the emerging residential street of Wellstones.
- Locally-listed buildings are important to Watford, and should not be carelessly replaced. However, the panel feels that proposed designs for the block fronting onto the High Street are relatively sensitive in terms of scale and design, and provide continuity between its two listed neighbours. Subject to high quality detailing and materials, this building could justify the replacement of the existing buildings.
- While the panel feels that the approach to the arrangement of windows onto the High Street is potentially interesting, it considers that this design approach should be supported by a character analysis of the High Street as a whole.



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- In contrast, the taller element to the rear of the site sits uncomfortably within its context and it is likely to have a significant and negative visual impact. In particular, the long blank façades of the taller block could dominate both the High Street and Wellstones.
- The panel is concerned by the lack of visualisations provided showing the buildings in relation to the wider area. These should be produced to allow the proposals to be fully assessed.

### *Scale and height*

- The panel considers that the volume of development required on the site exceeds its capacity. This has resulted in a proposal that is too tall for the context, and fills the entire site footprint, with overly complex floor plates, and the provision of limited daylight both for the units and the amenity space.
- The proposed rear element is considerably taller than any of its neighbours, including the Telephone Exchange, and is likely to be disruptive to the site's context.
- The panel is also concerned by the precedent that a 11-storey building will set for future development on Wellstones, and sees no justification in design terms for a building of this height in this location.
- While the height of the proposed rear tower element suggests it is intended as a point block, its width and two long blank façades means that, in fact, it will form part of sequence of buildings along Wellstones. The panel asks that greater clarity is developed about the intended approach to massing to ensure the building's presence in the streetscape is balanced and carefully designed.

### *Internal layout*

- The internal layouts of the units suggest that many will have dark interiors, and result in residential accommodation of questionable quality. The panel feels that the layout of each unit should be interrogated individually, to ensure that the quality of the spaces being created is acceptable.
- The scheme as a whole also borrows much of its daylight from the neighbouring sites, which could prejudice future development. The panel therefore asks that a full and rigorous year-round daylight analysis is undertaken, including all units and amenity spaces.

### *Entrances*

- The panel understands the rationale for not providing a residential entrance to the scheme from the High Street. The location of the single residential entrance on Wellstones is not in itself an issue, as the panel recognises that the character of this street will change.



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- However, the route from this entrance to the units in the front block is circuitous, and the quality of the entrance itself is poor, offering no sense of arrival. The entrance is subordinate both to the bin store and to the service entrance to the commercial unit.
- The narrowness of the entrance corridor (at 150cm wide) is a further concern, as it is likely to feel cramped and uncomfortable to use.

### *Other issues*

- The panel also raises its concern that no affordable housing is to be provided within the scheme, and that no accessible units are planned, and suggests that this approach requires further justification.
- It is important that the design team provides full data describing the scheme's environmental performance, so that it can be assessed as part of the design.

### *Next Steps*

- The panel looks forward to reviewing a further iteration of the scheme, including contextual visualisations, daylight analysis and environmental performance data, in due course.





**Watford Place Shaping Panel**

**Report of Chair's Review Meeting: 46 – 50 High Street**

Tuesday 12 January 2021  
Zoom video conference

**Panel**

Peter Bishop (chair)  
Anna Bardos

**Attendees**

Paul Baxter	Watford Borough Council
Sian Finney-MacDonald	Watford Borough Council
Ben Martin	Watford Borough Council
Louise Holman	Watford Borough Council
Alice Reade	Watford Borough Council
Tom Bolton	Frame Projects
Miranda Kimball	Frame Projects
Penny Nakan	Frame Projects

**Apologies / report copied to**

Andrew Clarke                      Watford Borough Council

**Confidentiality**

This is a pre-application review, and therefore confidential. As a public organisation Watford Borough Council is subject to the Freedom of Information Act (FOI), and in the case of an FOI request may be obliged to release project information submitted for review.

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## 1. Project name and site address

46 and 48-50 Watford High Street, Watford WD17 2BS

## 2. Presenting team

Jim Richards  
Jessica Wilson

Nissen Richards Studio  
Newsteer

## 3. Planning authority briefing

The site is located within Watford town centre. The frontage to the High Street consists of two locally listed buildings, which form part of a wider group of listed and locally listed buildings that contribute significant group value to the character of this part of the High Street. The site backs onto Wellstones to the rear, which is a public highway but operates principally as a rear service road to High Street properties.

Adjoining the site to the south-east is a recently completed mixed-use development including two commercial units fronting the High Street and 56 flats (Nos. 52A-56, High Street). The scale of the building is four storeys on the High Street and seven storeys on Wellstones. A similar scale of development has also recently been approved at No. 60, High Street adjoining Nos. 52A-56. To the west of Wellstones is the Telephone Exchange site, which consists of a large exchange building and extensive surface-level car parking.

This scheme was previously reviewed at a Small Sites Review Panel in August 2020 and returns for a Chair's Review to consider key amendments. The panel raised a number of concerns and since then, during the pre-application stage, the applicant and officers have worked to address these. Officers asked for the panel's comments, in particular, on:

- demolition of the high street building and storey building to High Street and change to commercial ground floor and flats
- overall height of the Wellstones block - 11 storeys reduced to eight storeys, including design and material amendments to the top floor, flanks and balconies
- reduction in number of residential units (from 31 to 25)
- new contextual views
- amendments to the entrance area from Wellstones
- sunlight and daylight assessment for the scheme



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## 4. Panel's views

### *Summary*

The panel welcomes amendments made in response to issues raised at the previous review meeting and feels the scheme has improved greatly as a result. The reduction in height of the taller element significantly improves its impact in views, and it now sits comfortably within its local context. The redesigned Wellstones entrance has greatly improved the experience for residents. The panel suggests that, rather than using a contrasting cladding material for the top storey of the Wellstones block, brick should be considered as an option. The applicant should ensure daylight levels will remain sufficient if the neighbouring plot, at No. 58 High Street, is developed in future. Overall, the panel feels the scheme will be a high quality addition to the High Street but deems it critical that the current architect remains involved through to completion, to ensure the proposed quality detail design is delivered. It encourages officers to condition this as part of any consent. These comments are expanded below.

### *Scale and height*

- The panel welcomes the reduction in height of the scheme's taller rear block from 11 storeys down to eight storeys, and feels it represents a significant improvement from the designs presented at the August 2020 Place Shaping Review. Key views now show that the revised scheme integrates well into the local context.

### *Daylight*

- The panel notes the importance of ensuring that, in the event of a development on the neighbouring site at 58 High Street, the loss of the light provided by secondary windows will not have a damaging impact on overall levels of daylight within the building.

### *Entrance area and servicing*

- Overall, the panel feels the re-design of the entrance to the development from Wellstones represents a significant improvement on the previous approach.
- The panel welcomes the removal of the loading bay from Wellstones and supports the servicing of the commercial units from the High Street.
- The entrance hallway will require careful day-to-day management to prevent clutter from accumulating and undermining the quality of a space that is very important to the experience of residents. The panel suggests that the application's reserved matters address this issue.

### *Materiality and quality*

- The panel supports the use of high quality materials across the two blocks. However, it does not consider that the top floor of the Wellstones block needs



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to reflect the anodized metal work used for the mansard roof of the four-storey High Street block.

- The panel encourages the applicant to re-visit the material treatment on the eighth storey and to test the alternative option of carrying brickwork to the top of the rear block. The current material choice does not reduce the appearance of bulk, and a single material may result in a stronger overall form.
- It also notes that it is simpler to deliver brickwork detailing to a high standard than to do so with cladding, which can appear low quality if poorly installed.
- The panel considers that the podium courtyard has improved since the last Place Shaping Panel. It encourages the applicant to invest in this space and its landscaping as an important element in delivering a quality scheme.
- The panel acknowledges the work on the brick detail on the flank wall and rear elevation since the previous review and feels this area of the scheme had made a big step forward.
- The quality of the materials and details across the scheme, including windows, insets, brick detail and fascia, are critical to its success. The panel also considers it essential that the current design team remains involved in the scheme through to completion to ensure the proposed quality of design is delivered. It recommends that officers include this as a condition of any planning permission.

### *Commercial units*

- The success of the scheme will be partly determined by whether the commercial units fronting onto the High Street are occupied once completed. The panel supports the applicant's efforts to ensure the space provided meets the needs of potential future occupiers.

### *Sustainability*

- The panel points out the value of analysing the whole life carbon embodied in the scheme and asks the applicants to provide this information if at all possible and use it to refine designs.

### *Next steps*

- The panel supports the revisions to 46-50 High Street and, encourages the applicant to continue to refine the design in collaboration with Watford officers.



## Agenda Item 5

<b>Committee date</b>	Tuesday 7 September 2021
<b>Application reference</b> <b>Site address</b>	21/00698/FULM - Marchwood House, 934 - 974 St Albans Road, Watford, WD25 9NN
<b>Proposal</b>	Redevelopment of the site to provide 127 residential units in 5 buildings ranging from 3 to 5 storeys in height, with associated parking, communal landscaped amenity areas, secure cycle parking and other associated development.
<b>Applicant</b>	Fairview New Homes (London) Limited
<b>Agent</b>	Fairview New Homes (London) Limited
<b>Type of Application</b>	Full Planning Permission
<b>Reason for committee Item</b>	Major Application
<b>Target decision date</b>	Wednesday 8 September 2021
<b>Statutory publicity</b>	Watford Observer, Neighbour Letters and Site Notice
<b>Case officer</b>	Andrew Clarke, andrew.clarke@watford.gov.uk
<b>Ward</b>	Meriden

### 1. Recommendation

- 1.1 That planning permission be granted subject to conditions, as set out in section 8 of this report.

### 2. Site and surroundings

- 2.1 The site is not in active use comprising the former Garston Bus Garage with its locally listed administration building fronting directly onto St. Albans Road. It also includes a former garage block, a substantial industrial building occupying the majority of the rear of the site with large areas of associated hardstanding.
- 2.2 The site is bordered by Garston Park on one side and to the rear by the gardens of 2 storey post war residential properties along Codicote Drive. There are two existing accesses directly onto St. Albans Road. The southernmost access abuts the 2 storey locally listed Garston Park Parade, set out around the north eastern side of the junction with Garston Lane. It contains retail and other commercial properties with residential accommodation above.
- 2.3 To the east of the site lies a large and predominantly residential estate. Developments of streets, avenues, and cul-de-sacs of semi-detached and detached 1-2 storey houses has taken place along with some flatted

developments. These estate properties demonstrate a high degree of architectural uniformity and a consistent pattern of estate style development.

- 2.4 To the west are flatted residential blocks, typically 3 storeys with rear and front parking courts, opposite the site. There are also two single storey fast food outlets with front parking areas, a place of worship and a hotel.
- 2.5 This upper section of St Albans Road has a mixed character with a strong setting of suburban residential development. There is variety in building type, form, size, and function. This is commensurate with its location close to a major transport route and on a busy thoroughfare into and out of Watford Town Centre.

### **3. Summary of the proposal**

#### **3.1 Proposal**

- 3.2 Redevelopment of the site to provide 127 residential units in 5 buildings ranging from 3 to 5 storeys in height, with associated parking, communal landscaped amenity areas, secure cycle parking and other associated development.

#### **3.3 Conclusions**

- 3.4 The redevelopment of the former Garston Bus Garage to create a high quality residential development is considered acceptable in principle.
- 3.5 The loss of the locally listed administration building is justified by the benefits of the proposed redevelopment which would deliver a well-designed residential development of a modern architectural style with cues taken from the existing 1950's modernist style of the existing building. Prior approval has already been granted for its demolition.
- 3.6 The layout, scale and design of the proposal is supported, optimising the potential of the site for new housing, echoing the current architectural vernacular and relating well to the surrounding context.
- 3.7 The mix and quality of the proposed accommodation is supported with all units benefiting from private amenity space and good internal daylight levels.
- 3.8 The lack of any affordable housing or contribution is a result of the marginal viability of the proposals, which has been verified by the Council's own viability advisors. However, a review mechanism is agreed that would allow

financial payment to be made towards affordable housing provision should the viability of the development be shown to have improved over time to provide a financial surplus.

- 3.9 The separation distances of the proposed buildings from the surrounding built environment is acceptable and ensures no significant harm to neighbouring amenity in terms of loss of light, privacy or outlook in compliance with the Residential Design Guide.
- 3.10 The site is well served by public transport. The parking provision, electric vehicle charging points, ample cycle storage and on site car club provision meet the aspirations of the draft Local Plan which encourages more sustainable transport modes.
- 3.11 The proposal would deliver a significant uplift in biodiversity with large areas of soft landscaping which include the planting of 80 trees throughout the site.
- 3.12 The inclusion of Air Source Heat Pumps and Solar Photovoltaics would mitigate carbon emissions in line with the policies of the draft Local Plan.
- 3.13 The air quality impacts from dust generated by demolition, earthworks, and construction would not be significant and the emissions generated from the completed development would be negligible.
- 3.14 In summary, the proposals constitute an appropriate reuse of a vacant brownfield site to provide much needed new housing for Watford. The proposals would not give rise to any significant planning harm and therefore approval is recommended.

#### **4. Relevant policies**

- 4.1 Members should refer to the background papers attached to the agenda. These highlight the policy framework under which this application is determined. Specific policy considerations with regard to this particular application are detailed in section 6 below.
- 4.2 Paragraph 11 d) of the National Planning Policy Framework 2021 (NPPF) establishes the 'presumption in favour of sustainable development' and the principles of the 'tilted balance' that apply where a local planning authority cannot demonstrate a 5 year housing supply or have failed to deliver at least 75% of their housing requirement as part of the Housing Delivery Test. Where the tilted balance applies, decision makers should grant permission unless NPPF policies on protected areas or assets of particular importance provide a

clear reason for refusing development or, any adverse impacts of granting permission would significantly and demonstrably outweigh the benefits, assessed against NPPF policies taken as a whole. The tilted balance has the effect of shifting the weight in the planning balance away from local policies and towards the NPPF.

- 4.3 The Council cannot currently demonstrate a 5 year supply of housing and scored below 75% in the most recent Housing Delivery Test results and therefore the 'tilted balance' applies to the determination of this planning application.

## **5. Relevant site history/background information**

- 5.1 An earlier proposal by the same applicant to redevelop the site to provide 165 residential units was refused at the Development Management Committee on 16<sup>th</sup> December 2019 (ref. 19/00496/FULM). The Committee refused the application due to the loss of the locally listed building, the nature of the design and scale of the buildings and encroachment of the established building line on St Albans Road. The applicant appealed the refusal to the Planning Inspectorate. The refusal was upheld and the appeal was dismissed on 15<sup>th</sup> October 2020.
- 5.2 The planning inspector's report concluded that the proposal would fail to take the opportunity to promote a high quality of design that responds to its location and would cause considerable harm to the character and appearance of the area. The locally listed administration building was considered to have low significance and its loss carried no more than a small amount of weight against the proposal.
- 5.3 Following the appeal, the applicant appointed a new architect who developed this new proposal with pre-application discussions commencing in January 2021. The new proposal was presented to the Council's Place Shaping Panel twice. The Panel made several recommendations. As result, amendments were made to the scheme and this application was received in May 2021.
- 5.4 In August 2021 prior approval was granted for the demolition of the existing buildings (ref.21/01185/DEM). Prior approval is only required in so far as the Council may wish to agree the method of demolition of the existing building. Demolition of the existing building does not require planning permission.

## **6. Main considerations**

- 6.1 The main issues to be considered in the determination of this application are:

- (a) Principle of a residential development
- (b) Loss of the locally listed building
- (c) Layout, scale and design
- (d) Accommodation
- (e) Affordable housing provision
- (f) Impact on amenity of adjoining residential properties
- (g) Transport, parking and servicing
- (h) Trees and biodiversity
- (i) Sustainability
- (j) Environmental impacts
- (k) Section 106 and Community Infrastructure Levy (CIL)

6.2 (a) Principle of a residential development

Policy HS1 of the Core Strategy lists the criteria that will be taken into account in assessing the suitability of sites for residential use. In this case, the site is brownfield land, is close to a neighbourhood centre, has good public transport connections, has no significant biodiversity value and is not at significant risk of flooding. These criteria would support a residential development in this location.

6.3 The administration building fronting St Albans Road is locally listed. There are no other planning designations on this site. The loss of the bus depot, which has been vacant since 2018 is acceptable. The principle of a residential development would be in keeping with the residential nature of the area.

6.4 (b) Loss of the locally listed building

The administration building fronting St Albans Road is a locally listed non-designed heritage asset. As such, it does not attract the same level of protection in policy or statute that is given to statutorily listed buildings. This two storey brick building has a 1950's modernist style with its principal elevation recessed at ground floor with 10 slim windows evenly spaced at first floor level between two projecting bays. The brick is predominantly Buckinghamshire red multi in a diamond pattern with blue engineering bricks used at ground floor level. The building was constructed in the early 1950's to a design by the chief architect to the London Transport Executive, Thomas Bilbow, who is better known for designing the Grade II Listed bus garage at Stockwell in London.

6.5 Paragraph 203 of the NPPF requires the effect of an application on the significance of a non-designated heritage asset to be taken into account, and advises that, a balanced judgement will be required, having regard to the scale of any harm or loss and the significance of the heritage asset.

- 6.6 In the recent planning appeal the planning inspector commented that the building has limited landmark value and is not a rare or special example of a post war bus garage being utilitarian in its appearance. The inspector also considered the potential of reusing the building as limited given that it was not designed for residential use, and considered that there was negligible group value in the subject building and the adjacent locally listed shopping parade. It was concluded that the level of interest of the building in architectural, historic, and archaeological terms was low at best and the inspector came to the balanced judgement that the loss of this building carries no more than a small amount of weight in terms of the planning balance.
- 6.7 Given the appeal history, the loss of the administration building is justified by the proposed redevelopment which would deliver a well-designed residential development which echoes the existing 1950's modernist style. The harm resulting from demolition of the locally listed building would be partly mitigated by a condition requiring a detailed historic building record be made prior to demolition.
- 6.8 The applicant has obtained prior approval for the demolition of the administration building. The demolition of non-designed heritage assets does not require planning permission, only prior approval which allows the Council to agree the method of demolition and the restoration of the site.
- 6.9 (c) Layout, scale and design  
The proposed layout of the site would comprise three lines of buildings orientated on a north south axis across the site. The building fronting St Albans Road would be 4 storeys in height, the building to the middle of the site would be 5 storeys in height and the three buildings to the rear of the site would be 3 storeys in height.
- 6.10 The existing northern vehicular access would be retained, albeit with some realignment including a new ghost island to the middle of the St Albans Road to allow northbound vehicles to turn into the site without blocking the northbound carriageway. The internal street would be a U shaped cul-de-sac passing along the north of the site tuning between the middle and rear buildings, before turning along the south of the site to a turning head which would allow vehicles to turn around and leave the site.
- 6.11 To either side of the street would be car parking spaces interspersed with trees and soft landscaping. The extent of hard landscaping and the dominance of car parking through the centre of the site has been a concern of planning officers and Watford's Place Shaping Panel during the development of the

current proposals. The applicant has made improvements to the proposal which reduces hard landscaping and increases soft landscaping to address these concerns, though the footprint and massing of the buildings remained unchanged. Seeking to make a more optimal use of land should not be at the expense of the quality of the public realm. On balance it is considered that the extent of hard landscaping is considered acceptable, retaining an acceptable quantum of amenity space and soft landscaping whilst ensuring parking provision is sufficient to serve the development.

- 6.12 The site would have three access points which could be used by pedestrians and cyclists. This includes a direct access into Garston Park, linking into the existing network of paths which were upgraded in 2020. A contribution of £10,966.55 towards these upgrade works, which include a new pathway to the site boundary, would be secured through a s106 agreement. The access point to Garston Park within the proposed development has a legible position at the end of the street between the buildings and would be well overlooked.
- 6.13 The space between the front and middle buildings would be a large central landscaped communal amenity space for residents of the proposed development with incidental play space, benches and a variety of trees. To the rear of the rear buildings adjacent to the eastern boundary of the site would be additional private communal amenity space for use by occupants of the rear buildings.
- 6.14 The massing of the front building responds to the context of St Albans Road where flatted development of a similar height, albeit with 3 storeys and pitched roofs exist. The footprint of the proposed building is set further back and its depth and height are significantly less than that proposed by the previous application. These adjustments to the footprint and the massing are considered to sufficiently overcome the concerns raised regarding the previous proposal by the Council and the planning inspector who concluded that the transition between the proposal and the adjacent locally listed shopping parade was abrupt and unsympathetic. This proposal would be more sympathetic to the context on both approaches to the site along St Albans Road and is considered acceptable.
- 6.15 The building to the middle of the site is the tallest at 5 storeys. Positioning the tallest element of the proposal in the middle of the site is a sensible solution optimising the potential of the site to provide much needed new homes without causing significant harm to the street scene of St Albans Road or to the amenity of the surrounding residential properties.

- 6.16 To the rear, a line of three, 3 storey buildings, separated by 1.8 metre gaps would have a townhouse aesthetic which is sympathetic to the 2 storey post war residential properties along Codicote Drive.
- 6.17 To the north east corner of the site would be an electricity substation building. This substation currently exists within the large rear garage building. Its position is unchanged.
- 6.18 The architectural approach has evolved from a historical and contextual analysis of the site. The front building would be articulated by a pre-cast concrete stepped parapet with strong projecting bay elements. Many of the lower windows would be framed by pre-cast concrete reveals with blue brick insets. The windows would be slim with a distinct vertical emphasis which relates well to the façade of the building. Externally the predominant material would be brown multi brick with blue brick used to provide architectural interest. The window frames and balconies would be dark grey. The elevations would have a simple, yet detailed visual appearance which echoes the 1950's modernist design of the locally listed administration building. This design approach is considered to be a significant improvement over the previous proposal which was described by the planning inspector as 'wholly uninspiring and unappealing'.
- 6.19 The middle building would have a similar design to the front building with a horizontal parapet line with raised gable elements. The material palette would also include a red multi brick alongside the brown multi brick.
- 6.20 The three rear buildings also exhibit a similar design to the middle building without a setback top floor seen on the other buildings. The style and proportions of these buildings are more akin to the appearance of townhouses. The projections on these buildings are shallower and topped with raised gable elements which add further architectural interest.
- 6.21 The proposal was twice subject to independent design review by Watford's Place Shaping Panel in March and May 2021. The panel's reports are attached at appendices 2 and 3. The panel recognised that the proposals had improved at the second review. In response to comments received from these reviews the applicant made a number of amendments to the proposal including:
- Improvements to the northern street which included narrowing the roadway, widening the soft landscaping and improving the connection to Garston Park.
  - Reconfiguration of parking spaces, reducing the overall number of spaces by 2 and moving a further 2 spaces from the north to the south of the site.

- Internal reconfiguration of the middle building to increase the number of dual aspect units by 8.
- Moving balconies from the front elevation to the side elevations on the front building.
- Simplified materials palette for the front building.
- Improved landscaping and enclosure fronting St Albans Road.
- Increased privacy for units facing the central amenity space.
- The introduction of Air Source Heat Pumps in place of individual gas boilers.

#### 6.22 (d) Accommodation

The development would provide the following mix of accommodation:

- 44 x 1 bedroom units (34.5%)
- 64 x 2 bedroom units (50.5%)
- 19 x 3 bedroom units (15%) (including 4 x duplex units)

6.23 The proposed mix is considered appropriate in accordance with Policy HS2 of the Core Strategy which seeks a variety of housing typologies.

6.24 Section 7.3.6 of the Residential Design Guide sets out the minimum Gross Internal Areas for new dwellings in accordance with the Nationally Described Space Standards (NDSS). All of the proposed units would meet the minimum floorspace standard for the dwelling type proposed and are compliant with the NDSS. All of the units include private amenity space in the form of private gardens at ground floor and balconies and terraces on the upper levels. All balconies exceed a depth of 1.5 metres and have a footprint in excess of 5 square metres.

6.25 All ground floor units have direct external access, in some units this is the sole primary access and in others it is a secondary access from the living rooms. Six of the ground floor units are suitable for occupants with wheelchairs. These units have larger internal spaces and all benefit from direct external access into the units.

6.26 The development includes 47 (37%) single aspect units of which 40 are one bedroom units. 25 units are solely east facing and 22 are west facing. Having no single aspect north or south facing units facilitates good sunlight levels and avoids excessive solar gain.

6.27 A daylight/sunlight assessment following the British Research Establishment's (BRE) best practice guidance "Site Layout Planning for Daylight and Sunlight. A Guide to Good Practice." (2011) has been undertaken. This assessment

concludes that in terms of Average Daylight Factor (ADF), a qualitative measure of the adequacy of diffuse daylight within rooms, all habitable rooms would meet or exceed the minimum BRE recommendations. Overall, the proposed levels of daylight and sunlight within the development are good.

- 6.28 (e) Affordable housing provision  
Policy HS3 of the Core Strategy requires a 35% provision of affordable housing. This provision should have a tenure mix of 65% affordable rent, 20% social rent and 15% intermediate tenures. Draft Local Plan Policy HO3.3 also requires 35% provision, with a tenure mix which includes 60% social rent and 40% intermediate tenures.
- 6.29 The submitted application proposes no affordable housing. This has been justified through the submission of a detailed viability appraisal which shows the development to be unviable, generating a significant deficit of around £4.59 million. On this basis, the applicant concludes that the scheme cannot provide any on-site affordable housing. This is based on a profit level of 18% of Gross Development Value. This is considered to be reasonable and within the guideline range of 15-20% within the government's Planning Practice Guidance.
- 6.30 Vacant Building Credit (VBC) was introduced into the July 2018 version of the NPPF. In the 2021 version it is referenced at paragraph 64. Where vacant buildings are being reused or redeveloped, VBC allows any affordable housing contribution due to be reduced by a proportionate amount, relative to floorspace. The applicant has applied VBC which would allow the 35% provision required by Policy HS3 to be reduced to 11.35%.
- 6.31 The applicant's viability appraisal has been subject to a detailed and robust viability review by Aspinall Verdi (AV), acting on behalf of the Council. AV tested a policy-compliant scenario to determine whether the scheme could support the contribution sought by Policy HS3 (equivalent to 11.35% with VBC considered). The outcome of this policy compliant scenario concluded that the development would still be unviable generating a deficit of £3.67 million.
- 6.32 AV also tested an entirely private scenario to determine whether the scheme would be financially viable. This concluded a deficit of £2.25 million, some £2.34 million less than the deficit generated in the applicant's appraisal. Nevertheless AV conclude that despite favourable adjustments to the Benchmark Land Value (BLV) and construction costs, an entirely private scheme remains unviable at the present time. Notwithstanding these conclusions, and in recognition of the need for affordable housing in the borough, the applicant has made a without prejudice offer of a commuted

sum of £194,000 towards affordable housing. This is a welcome benefit and is accepted in the circumstances and is in addition to a late stage review mechanism (see below).

- 6.33 Over time values fluctuate. AV have shown that if sales values increase and construction costs decrease, a policy compliant scheme begins to become viable. For example, with a 5% increase in sales values and a decrease in construction costs by the same amount, the scheme generates a surplus of around £418,000 above the BLV and can support the full contribution sought by Policy HS3 with VBC applied. Conversely, if values were to decrease by 5.0% and costs were to increase by the same amount, the deficit would be further exacerbated to around £6.45m.
- 6.34 AV have strongly recommended that a viability review mechanism is included within any Section 106 agreement. This should be undertaken towards the end of the development. The Council and the applicant have agreed to a review mechanism to be secured through a s106 agreement. This will allow any positive change in the viability of the scheme to be captured and where viable, an appropriate additional commuted sum towards affordable housing to be secured.
- 6.35 (f) Impact on amenity of adjoining residential properties  
Park Lodge, a development of 20 one bedroom flats with a communal rear garden, adjoins the northern boundary of the site. The southern side wall of this development is approximately 0.5 metres from the northern boundary of the site and contains non-habitable room windows. Proposed Building A does include north facing habitable room windows and balconies on the north end of the building. These are all more than 14 metres from the shared boundary which is sufficient to ensure no unreasonable loss of light and privacy to the side elevation of Park Lodge.
- 6.36 The rear gardens of numbers 63 to 83 (odds) Codicote Drive adjoin the eastern boundary of the site. These 2 storey properties have gardens which extend approximately 20 metres beyond the original rear wall of the dwellings. Currently the substantial industrial building occupying the rear of the site is approximately 2.5 metres from the common boundary with these properties. This building is clearly visible above their rear fences, though many gardens contain tall mature trees which somewhat obscure this view. Proposed Buildings E, F and G are all three storeys in height and positioned 13.5 to 14 metres from the rear boundary except for ground floor single storey protrusions which extend 3 further metres towards the boundary. The two first floor level terraces on Buildings E and G are 11 to 11.5 metres from

the common boundary. The ten balconies at first and second floor levels are 12 to 12.5 metres from the same boundary.

- 6.37 In order to maintain acceptable levels of privacy between the proposed development and existing residential properties the Residential Design Guide recommends a minimum distance of 11 metres to the rear boundary and 27.5 metres to rear facing habitable room windows. In relation to Codicote Drive, both are achieved. As such, the proposed buildings will not give rise to any unacceptable overlooking or loss of privacy to these properties and their private garden areas.
- 6.38 A number of properties at the closed end of Felden Close have rear garden boundaries which adjoin the southern boundary of the site. The side elevation of proposed Building G would be positioned 5 metres from the common boundary with number 34 Felden Close which has an 18 metre deep garden. This separation would not significantly harm the outlook of number 34 given that Building G is smaller and at a greater distance than the existing industrial building. A condition has been included to ensure windows in this side elevation are obscure glazed and the balconies have suitable privacy screens to their southern edge.
- 6.39 To the western side of the southern boundary are the properties which form part of the locally listed Garston Park Parade. The main Garston Park Parade terrace is orientated perpendicular to the common boundary with no side facing windows. Number 19 Garston Park Parade, an infill building which sits behind the parade adjacent to the boundary with the bus garage, has no first floor habitable room windows overlooking the site. The development would therefore not harm the amenity of the properties in Garston Park Parade.
- 6.40 The buildings on the opposite (west) side of St Albans Road would not suffer any loss of amenity given their generous separation across St Albans Road.
- 6.41 The applicant has submitted a daylight/sunlight report which assesses light levels to windows within adjoining properties in accordance with the Building Research Establishment's publication "Site Layout Planning for Daylight and Sunlight. A Guide to Good Practice." (2011). This report accurately demonstrates that the proposed development would cause no significant harm to any existing window.
- 6.42 Overall the separation distances of the proposed buildings from the surrounding buildings are acceptable, in compliance with the Residential Design Guide, which ensures no significant harm to neighbouring amenity in terms of loss of light, privacy or outlook.

- 6.43 (g) Transport, parking and servicing  
The site is served by frequent buses along St Albans Road, which connect the site to Watford Town Centre and other centres in Hertfordshire. Buses run at a daytime frequency of approximately every 10 minutes with journeys to the town centre taking around 15 minutes. Garston railway station is only 0.4 miles from the site. This station is served by a shuttle train every 45 minutes between Watford Junction and St Albans. The frequency of buses and trains is reduced at evenings and weekends, though overall the public transport connections to the site are relatively good.
- 6.44 The development would provide 237 cycle parking spaces within 7 internal ground floor rooms located adjacent to the main communal entrances. The easily accessible and secure nature of the cycle parking encourages people to choose cycling as their preferred mode of transport.
- 6.45 The development would also provide 87 surface level car parking spaces. Of these spaces 20% would have active charging infrastructure for electric vehicles provided, all other spaces would have passive provision. Five spaces are suitable for use by persons with a disability and 1 would be allocated for use by a car club.
- 6.46 The parking standards contained in saved Policy T22 of the Watford District Plan 2000 set maximum parking requirements for new developments. For the proposed development the maximum car parking provision would be 193.75 spaces. Nevertheless these maximum parking standards are historic and out-of-date and are not reflective of current aspirations to encourage the use of more sustainable transport modes. Policy ST11.5 of the draft Local Plan states the maximum requirement in this area to be 1 space per unit (equating to 127 spaces). The proposed parking ratio of 0.69 spaces per unit is within these maximum standards and considered appropriate given the alternative sustainable transport options available.
- 6.47 The Transport Assessment submitted with this application presents car ownership data which suggests that parking provision is sufficient to cater for the likely demand associated with the development. Nevertheless a parking survey of nearby streets has been undertaken which suggests there is adequate available space on-street to accommodate any additional demand.
- 6.48 To ensure the needs of future occupiers and encourage sustainable development details of car parking allocation and distribution are secured by condition. As part of the s106 agreement a financial payment to Hertfordshire

County Council of £6,000 is secured for the long term monitoring of a Travel Plan which further encourages sustainable travel options.

- 6.49 The existing site has two vehicular access points, both onto St Albans Road. Hertfordshire County Council Highway Authority have agreed to the realignment of St Albans Road, subject to a 278 Highways Act agreement. The northern vehicular entrance would be retained, albeit with some realignment as noted in section (c) above. The southern entrance would be closed with a kerb line established. The closure of the southern entrance as opposed to the northern one is considered preferable from a highways safety perspective. Internally, the street is wide enough for vehicles to pass and for large refuse collection vehicles to access the bin stores. The southern turning head has been designed to allow large vehicles to turn around.
- 6.50 The Transport Assessment includes a trip generation exercise which compares the predicted vehicle trip generation against that of the previous use as a bus garage. This exercise concludes that the number of arrivals in the morning peak would be fewer and the number of departures would be greater. Conversely arrivals in the afternoon peak would be greater and the number of departures would be fewer. Overall the combined trip numbers generated by the proposed use would be fewer than that of the former use as a bus garage and the impact on the local highway network would be negligible.
- 6.51 (h) Trees and biodiversity  
The site and surroundings do not contain any trees protected by a Tree Preservation Order (TPO). To the front of the existing administration building is a small sized Ash tree which would be removed to facilitate this proposal. The four trees within the pavement to the front of the site, which do have prominence in the street scene would not be impacted by the proposal or the realignment of the highway.
- 6.52 The existing site has a small grass area to the front of the site, other than this the site is covered by buildings and hardstanding and has no significant biodiversity value. The proposed development includes the planting of 80 trees of varying species throughout the site with landscaped communal amenity areas. This represents a significant enhancement of the natural environment. The mature trees in the rear gardens of properties on Codicote Drive would not be impacted.
- 6.53 (i) Sustainability  
The development has been designed to use a hybrid heating and hot water system which utilises a combination of central gas fired boilers and an Air Source Heat Pump (ASHP) system. The ASHP is a renewable energy technology

which uses electricity to capture latent, low grade heat from the atmosphere and convert it into higher grade heat sufficient for heating. Solar Photovoltaics (PV) are also included on the roof of the front building to further increase sustainable energy generation and carbon savings.

- 6.54 Overall the proposal would achieve a target emission rate which is an improvement of 19% over that required by current national building regulations, in accordance with Policy CC8.3 of the draft Local Plan, which seeks a phased approach to improve the energy efficiency of new homes.
- 6.55 (j) Environmental impacts  
The applicant has submitted an Air Quality Assessment which assesses potential air quality impacts from dust generated by demolition, earthworks, and construction. These are predicted to be not significant. The highways in the vicinity of the site are not designated as an Air Quality Management Area (AQMA). Once completed the emissions generated by the development would be negligible.
- 6.56 The application is accompanied by reports detailing assessments in respect of contamination, flood risk, surface water drainage, ecology, energy, noise and biodiversity. These technical matters have been demonstrated to be acceptable to support the planning application and relevant requirements are secured by condition.
- 6.57 (k) Section 106 and Community Infrastructure Levy (CIL)  
The Council introduced the Community Infrastructure Levy (CIL) with effect from 1 April 2015. The CIL charge covers a wide range of infrastructure as set out in the Council's Regulation 123 list, including highways and transport improvements, education provision, youth facilities, childcare facilities, children's play space, adult care services, open space and sports facilities. CIL is chargeable on the relevant net additional floorspace created by the development. The charge is non-negotiable and is calculated at the time that planning permission is granted. The CIL charge applicable to the proposed development is c.£157per m<sup>2</sup> (including indexation).
- 6.58 Section 106 planning obligations can only be used to secure affordable housing provision and other site specific requirements. The planning obligations for this development are set out in Section 8 of this report.

## **7 Consultation responses received**

### **7.1 Statutory consultees and other organisations**

<b>Name of Statutory Consultee / Other Organisation</b>	<b>Comment</b>	<b>Response</b>
Hertfordshire Constabulary Crime Prevention Design Service	Requests the development is built to the police's security standards "Secured by Design".	Development would comply with these standards.
Hertfordshire County Council Ecology	No response received.	None
Hertfordshire County Council Fire and Rescue Service	Suggested condition for fire hydrants.	Building regulations Approved Document B requires fire safety measures.
Hertfordshire County Council Flood Authority	Consulted. Sought additional flood risk assessment/ surface water drainage information. Additional information sent, awaiting response.	Pre commencement condition added to resolve this matter.
Hertfordshire County Council Growth and Infrastructure	Community Infrastructure Levy applicable.	Noted
Hertfordshire County Council Highways	No objection subject to s106 for Travel Plan, conditions and informatives.	Noted. A Travel Plan and monitoring contribution would be secured through a s106 agreement.
Hertfordshire County Council Historic Environment	Suggested a record is made of locally listed building.	Building record conditioned.
Hertfordshire County Council Minerals and Waste	General comments made. No objection.	Noted.
Thames Water	No objection.	Noted.

## 7.2 Internal Consultees

<b>Name of Statutory Consultee / Other Organisation</b>	<b>Comment</b>	<b>Response</b>
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Watford Borough Council Arboricultural Officer	No objection subject to conditions.	Noted.
Watford Borough Council Environmental Health	No objection subject to conditions.	Noted.
Watford Borough Council Housing Service	Disappointed that a scheme of 127 units cannot provide any affordable housing. Will follow the advice of the Council's viability advisors and planning officers.	Noted.
Watford Borough Council Waste and Recycling	Satisfied with proposal.	Noted.
Watford Borough Council Urban Design and Conservation Manager	Considered the proposal an improvement on the previous proposal.	Noted.

### 7.3 Interested Parties

Letters were sent to 247 properties in the surrounding area. A total of 24 responses were received in objection. The main comments are summarised below, the full letters are available to view online:

<b>Comments</b>	<b>Officer response</b>
Insufficient parking / increase in traffic	Parking provision is considered acceptable and the impact on the highway network is considered negligible. See paragraphs 6.45 to 6.50 of the report which address parking provision and highway impacts.
Overdevelopment of area	The development makes effective use of a brownfield site and contributes housing need in Watford. See paragraphs 6.2 to 6.3 which relates to the principle of a residential development.
Design, character and appearance of the area	The development is considered to respond well to the context. See paragraphs 6.9 to 6.21 of the report which relate to the layout, design and scale.
Overlooking / overshadowing / loss of	The development would cause no significant harm to neighbouring amenity. See paragraphs 6.35 to 6.42 of

light and loss of privacy.	the report which relates to the impact on the amenity of adjoining residential properties.
Pressure on local services	The development is CIL (Community Infrastructure Levy) liable. This levy is used to help the local authority to deliver the infrastructure needed to support development in the area.
Housing mix and lack of affordable housing	The housing mix is considered acceptable. The lack of affordable housing is disappointing, though this has been subject to an independent viability review. See paragraphs 6.22 to 6.23 and 6.28 to 6.34 of the report which relate to accommodation and affordable housing.
Loss of biodiversity	The site has limited biodiversity value. See paragraphs 6.51 to 6.52 of the report which relate to biodiversity.
Anti-social behaviour	The proposal is not considered to give rise to anti-social behaviour. The development is to be built to the Hertfordshire Constabulary's security standards.
Disruption from construction	A construction management plan to minimise disruption is conditioned. Air quality impacts from construction would be negligible. See paragraphs 6.55 to 6.56 of the report which relate to environmental impacts. The Environmental Protection Act, the Control of Pollution Act and the Highways Act also control the matters of disruption raised.
The ethics of the applicant	This is not a material planning consideration.

## 8 Recommendation

### Section 106 Heads of Terms

(i) To secure a financial payment to Watford Borough Council of £194,000 towards the provision of affordable housing within the borough;

(ii) To secure a review mechanism of the viability of the development to be undertaken towards the end of the project (at 75% of flat sales) when actual build costs and sales values of the flats are known. This shall allow financial payment to be made towards local affordable housing provision where the viability of the development can be shown to have improved to provide a financial surplus;

(iii) To maintain the 1 parking space for use of a car club, to facilitate the arrangement of an on-site car club and to provide funding for the membership of each dwelling for this car club for 3 years membership from the date of first occupation. At the point of occupation, should the car club be reasonably demonstrated as not viable, a payment of the equivalent cost of the car club shall be made to Watford Borough Council for contributions towards alternative sustainable transport infrastructure;

(iv) To secure a financial payment to Hertfordshire County Council of £6,000 for the long term monitoring of the proposed Travel Plan for the site;

(v) To secure a financial payment to Watford Borough Council of £10,966.55 for the connecting path through Garston Park to the site.

### Conditions

#### 1. Time Limit

The development to which this permission relates shall be begun within a period of three years commencing on the date of this permission.

Reason: To comply with the requirements of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

#### 2. Approved Drawings and Documents

The development hereby permitted shall be carried out in accordance with the following approved drawings and documents:

- T10100 Rev: P01
- T10200 Rev: P01
- T10201 Rev: P01
- T20100 Rev: P02
- T20101 Rev: P01
- T20102 Rev: P01
- T20103 Rev: P01
- T20104 Rev: P01
- T20105 Rev: P01
- T20200 Rev: P01
- T20201 Rev: P01
- T20202 Rev: P01
- TA20100 Rev: P01

- TA20101 Rev: P01
- TA20102 Rev: P01
- TA20103 Rev: P01
- TA20104 Rev: P01
- TC20100 Rev: P01
- TC20101 Rev: P01
- TC20102 Rev: P01
- TC20103 Rev: P01
- TC20104 Rev: P01
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- TE20102 Rev: P01
- TE20103 Rev: P01
- TA21400 Rev: P01
- TA21401 Rev: P01
- TC21400 Rev: P01
- TC21401 Rev: P01
- TE21400 Rev: P01
- TE21401 Rev: P01
- T70400 Rev: P01
- T70401 Rev: P01
- T70402 Rev: P01
- T70403 Rev: P01
- T90100 Rev: P02
- T91100 Rev: P01
- C0089 L100 Rev: U
- Sustainability and Energy Statement. Dated: April 2021. By Turley
- Noise Impact Assessment. Dated: April 2021. By Syntegra
- Arboricultural Implications Assessment. Dated: May 2019 Drafted by: CBA  
Trees
- Historic Building Record. Dated January 2020. By Icen

Reason: For the avoidance of doubt and in the interests of proper planning.

### 3. Flood risk assessment / surface water drainage strategy

No development shall commence until a flood risk assessment / surface water drainage strategy, which contains confirmation from Thames Water regarding the proposed discharge rates and volumes, justification of proposed pumped

connection and an appropriate SuDS management train, has been submitted to and approved in writing by the Local Planning Authority. The development shall only be implemented in accordance with the approved strategy.

Reason: In accordance with the response from the local lead flood authority to prevent flooding by ensuring the satisfactory storage of and disposal of surface water from the site in accordance with Policy SD2 of the Watford Local Plan Core Strategy and Chapter 14 of the National Planning Policy Framework.

#### 4. Construction Traffic Management Plan Addendum

No development shall commence until a Construction Traffic Management Plan Addendum has been submitted to and approved in writing by the Local Planning Authority. Thereafter the construction of the development shall only be carried out in accordance with the approved plan. The Construction Traffic Management Plan shall include details of:

- Construction vehicle numbers, type, routing;
- Access arrangements to the site;
- Traffic management requirements
- Construction and storage compounds (including areas designated for car parking, loading / unloading and turning areas);
- Siting and details of wheel washing facilities;
- Cleaning of site entrances, site tracks and the adjacent public highway;
- Timing of construction activities (including delivery times and removal of waste) and to avoid school pick up/drop off times;
- Provision of sufficient on-site parking prior to commencement of construction activities;
- Post construction restoration/reinstatement of the working areas and temporary access to the public highway;

Reason: In accordance with the response from the highway authority to protect highway safety and the amenity of other users of the public highway and rights of way in accordance with Policies 5, 12, 17 and 22 of Hertfordshire's Local Transport Plan (adopted 2018).

#### 5. Contamination Remediation Scheme

No development shall commence until the following components of a scheme to deal with the risks associated with contamination of the site shall each be submitted to and approved, in writing, by the Local Planning Authority:

(i) An options appraisal and remediation strategy, based on the site investigation results and the detailed risk assessment reported in the Phase I Desk Study Review and Phase II Ground Investigation Report prepared by Hydrock (Report ref. ABD-HYD-XX-XX-RP-GE-0001), giving full details of the remediation measures required and how they are to be undertaken.

(ii) A verification plan providing details of the data that will be collected in order to demonstrate that the works set out in (i) are complete and identifying any requirements for longer term monitoring of pollutant linkages, maintenance and arrangements for contingency action. Any changes to these components require the express consent of the Local Planning Authority. The scheme shall be implemented as approved.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

## 6. Contamination Verification Report

Prior to the occupation of each respective building, measures identified in the approved remediation scheme relating to that building shall have been completed and a verification report, which demonstrates the effectiveness of the remediation carried out together with any necessary monitoring and maintenance programme and copies of any waste transfer notes relating to exported and imported soils, shall be submitted to and approved in writing by the Local Planning Authority. The approved monitoring and maintenance programme shall be implemented in full.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

## 7. Unexpected Contamination

In the event that contamination is found at any time when carrying out the approved development that was not previously identified it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with the requirements of condition 5, and where remediation is necessary a remediation scheme must

be prepared in accordance with the requirements of condition 5, which is subject to the approval in writing of the Local Planning Authority. Following completion of measures identified in the approved remediation scheme a verification report must be submitted and approved in writing by the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

#### 8. External Materials

No external facing materials shall be installed on the building until:

- a. full details of the materials to be used for all the external surfaces have been submitted to and approved in writing by the Local Planning Authority; and
- b. sample panels shall be constructed on site to show the typical facades including brickwork, window frames and glazing made available for inspection and approved in writing by the Local Planning Authority.

Reason: In the interests of the visual appearance of the building and the character and appearance of the area, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

#### 9. Hard Landscaping

No part of the development shall be occupied until a detailed hard landscaping scheme for the site, including site boundary treatments and external lighting, has been submitted to and approved in writing by the Local Planning Authority, The detailed scheme shall be based upon the Illustrative Landscape Masterplan (drawing reference: - C0089 L100 Rev: U). No building shall be occupied until the respective works relating to the building have been carried out in accordance with the approved details.

Reason: In the interests of the visual appearance of the site and the wider area, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

#### 10. Soft Landscaping

No part of the development shall be occupied until a detailed soft landscaping scheme for the site and a landscape management and maintenance plan, has been submitted to and approved in writing by the Local Planning Authority. The detailed scheme shall be based upon the Illustrative Landscape Masterplan (drawing reference: C0089 L100 Rev: U). The approved soft landscaping scheme in respect of each building shall be carried out not later than the first available planting and seeding season after completion of the respective building. Any trees or plants whether new or existing which within a period of five years die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species, or in accordance with details approved by the Local Planning Authority.

Reason: In the interests of the visual appearance of the site and the wider area, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

#### 11. Highway Works

No part of the development shall be occupied until the modified access and egress arrangements from St Albans Road, the stopping up of the southern access to vehicles and the reinstatement of the footway / highway verge as shown in principle on the approved drawings have been completed in full.

Reason: In the interests of the safe operation of the site and the surrounding highway, in accordance with saved Policies T21 and SE7 of the Watford District Plan 2000.

#### 12. Car Parking Provision and Management Plan

No part of the development shall be occupied until a Car Parking Management Plan has been submitted to and approved in writing by the Local Planning Authority. The Car Parking Management Plan must include the following:

- Details of car parking allocation and distribution;
- Details of the car club regarding the operation, management, and implementation scheme;
- Methods to minimise on-street car parking;
- Monitoring required of the Car Parking Management Plan to be submitted to and approved in writing in accordance with a timeframe to be agreed by the Local Planning Authority;
- Provision of active EV charging spaces (at minimum 20%) and all other spaces to have passive infrastructure.

The Car Parking Management Plan shall be fully implemented before the development is first occupied, in accordance with a timeframe agreed by the LPA, and thereafter retained for this purpose.

Reason: In the interests of highway safety and to ensure sufficient on-site parking provision that meets the needs of future occupiers and encourages sustainable transportation.

### 13. Travel Plan

No part of the development shall be occupied until a detailed Travel Plan, based upon the Hertfordshire County Council document Framework Travel Plan and 'Hertfordshire Travel Plan Guidance', has been submitted to and approved in writing by the Local Planning Authority. The travel plan shall then be implemented in accordance with the approved scheme thereafter.

Reason: To ensure that the development offers a wide range of travel choices to reduce the impact of travel and transport on the environment, in accordance with Policy T3 of the Watford Local Plan Core Strategy 2006-31.

### 14. Bin and Cycle Storage

No dwelling within each building of the development shall be occupied until the bin and bicycle storage for that building has been provided for the use of residents, in accordance with the approved drawings. These facilities shall be retained at all times for the use of the residential occupiers of the dwellings.

Reason: To ensure that adequate facilities exist for the future occupiers of the dwellings, in accordance with saved Policies T10 and SE7 of the Watford District Plan 2000, Policy UD1 of the Watford Local Plan Core Strategy 2006-31 and the Residential Design Guide 2016.

### 15. Aerials and Satellite Dishes

No dwelling within each building of the development shall be occupied until details of any communal terrestrial television aerial(s) or satellite dish(es) for that building have been submitted to and approved in writing by the Local Planning Authority.

Reason: In the interests of the character and appearance of the buildings, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

## 16. Obscured glazing and privacy screens

Prior to the occupation of units 120 and 124 in Building G, details of obscured glazing and privacy screens to prevent overlooking towards the rear gardens of Felden Close shall be submitted to and approved in writing by the Local Planning Authority and shall be installed as approved. These measures shall be retained at all times.

Reason: To prevent overlooking and consequent loss of privacy to neighbouring premises pursuant to Policy UD1 of the Watford Local Plan Core Strategy 2006-31 and the Residential Design Guide 2016.

## 17. Communications Equipment

For the avoidance of doubt, no communications development permitted by Classes A, B or C of Part 16 of Schedule 2 of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) shall be undertaken on any of the buildings hereby approved unless otherwise approved in writing by the Local Planning Authority.

Reason: In the interests of the character and appearance of the buildings, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

## 18. Flat Roof not a Terrace or Balcony

No parts of the flat roofs of the development, with the exception of those areas marked as balconies or terraces on the drawings hereby approved shall be used as a terrace, balcony or other open amenity space.

Reason: To prevent overlooking and consequent loss of privacy to neighbouring premises pursuant to Policy UD1 of the Watford Local Plan Core Strategy 2006-31 and the Residential Design Guide 2016.

## Informatives

1. IN907 – Positive and proactive statement
2. IN909 – Street naming and numbering
3. IN910 – Building Regulations
4. IN911 – Party Wall Act
5. IN912 – Hours of Construction
6. IN913 – Community Infrastructure Levy Liability
7. IN915 – Highway Works – HCC agreement required





# Proposed Western Elevations

**Notes:**

1. Elevations shall show and include all elements.
2. Elevation shall include all elements.
3. The elevation shall be shown on the exterior of the building.
4. The elevation shall be shown on the exterior of the building.

**Material Key:**

- 1. Red Brick - American
- 2. Light Gray - Stone
- 3. Dark Gray - Stone
- 4. Dark Gray - Stone
- 5. Dark Gray - Stone
- 6. Dark Gray - Stone
- 7. Dark Gray - Stone
- 8. Dark Gray - Stone
- 9. Dark Gray - Stone
- 10. Dark Gray - Stone

**Scale:**

0 10 20 30 40 50

**Roife Judd**  
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Tulsa, Oklahoma 74116  
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**Project:**  
Fountain Park  
West End  
Tulsa, Oklahoma

**Client:**  
Fountain Park  
Tulsa, Oklahoma

**Architect:**  
Roife Judd  
Tulsa, Oklahoma

**Project No.:**  
10000

**Scale:**  
1/8" = 1'-0"

**West Elevation 1**

**West Elevation 2**

**West Elevation 3**

10000  
Scale: 1/8" = 1'-0"

# Proposed Side Elevations (northern and southern)

**NOTES**

1. All elevations are to be shown on all sides of the building.
2. All elevations are to be shown on all sides of the building.
3. All elevations are to be shown on all sides of the building.
4. All elevations are to be shown on all sides of the building.
5. All elevations are to be shown on all sides of the building.

**Material Key**

- 1. Brick
- 2. Concrete
- 3. Stone
- 4. Metal
- 5. Glass
- 6. Wood
- 7. Asphalt
- 8. Concrete
- 9. Brick
- 10. Concrete
- 11. Metal
- 12. Glass
- 13. Wood
- 14. Asphalt
- 15. Concrete

**North Elevation 1**

**South Elevation 1**

**Roife Judd**

1000 Grand Ave. | Grand Ave. | Portland, ME 04101  
 Phone: 603.633.1234 | Fax: 603.633.1235  
 Email: info@rojudd.com

**Project Name:** Faberwood New Homes

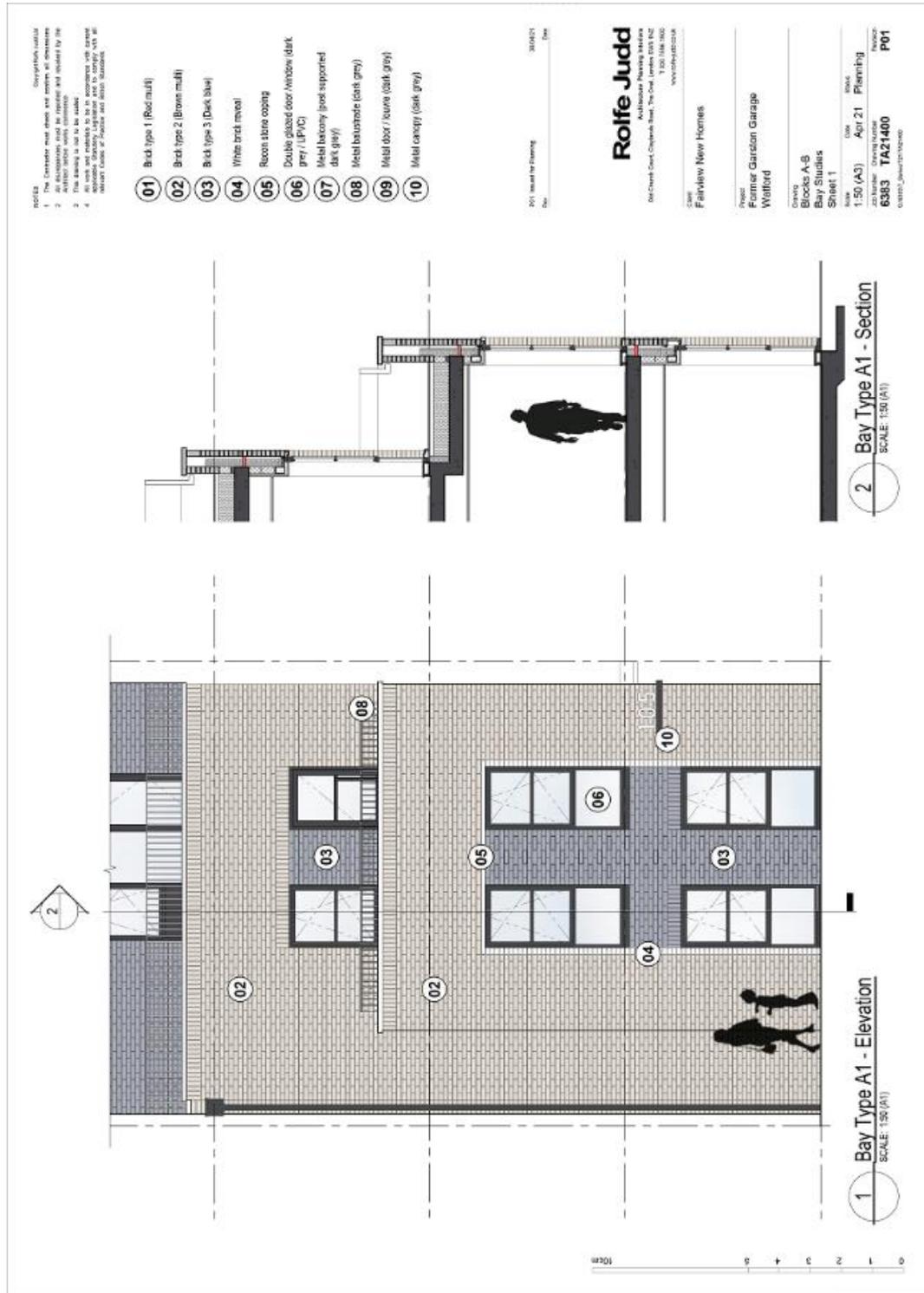
**Client:** Faberwood Construction

**Address:** Grand Ave. | Portland, ME 04101

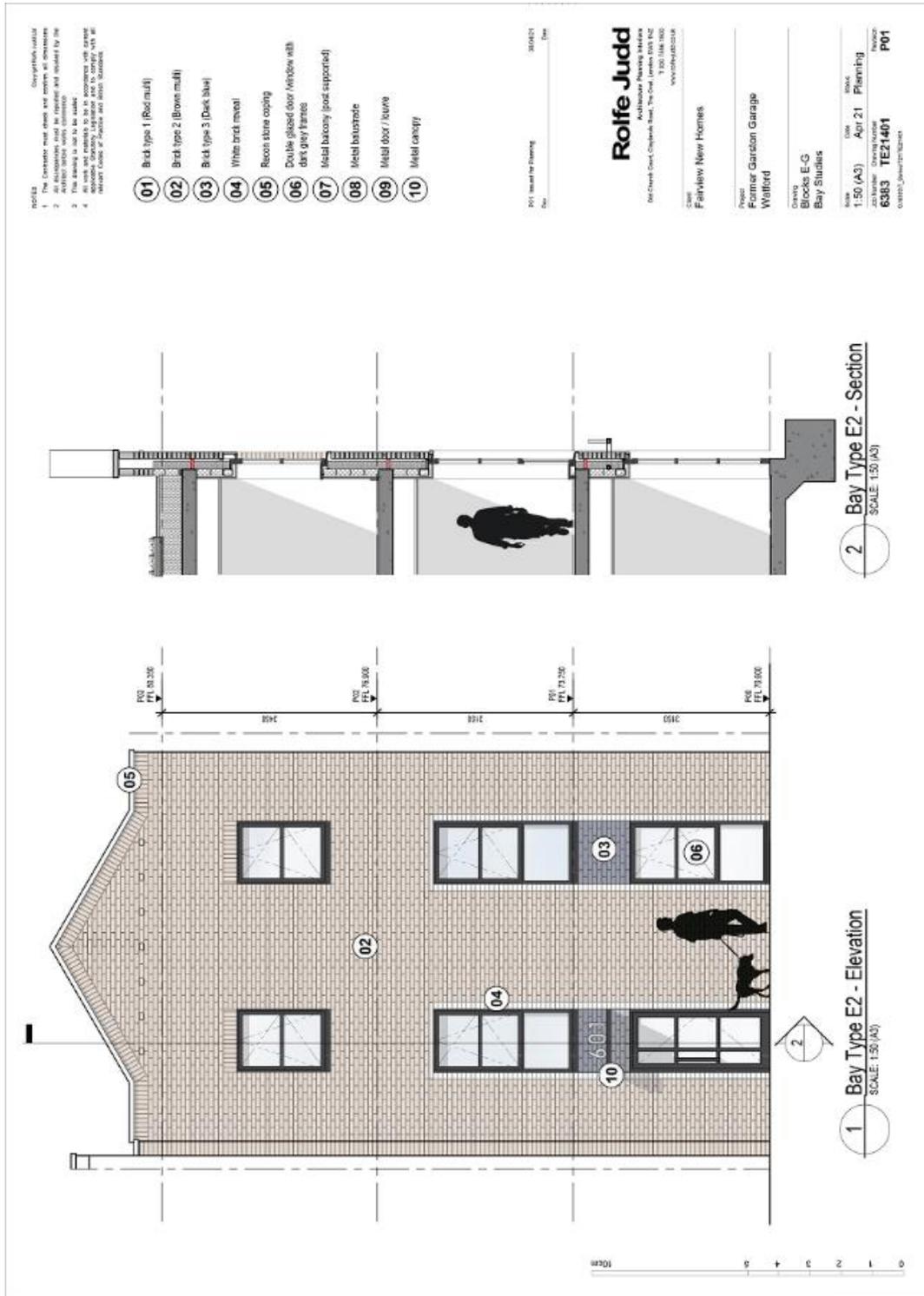
**Project No.:** RJ-2024-001

**Scale:** 1/8" = 1'-0"

# Proposed Bay Detail Drawing for Front Building



# Proposed Bay Detail Drawing for Rear Building









Visualisation from St Albans Road



Visualisation of proposed street looking towards Garston Park





**Watford Place Shaping Review Panel**

**Report of Formal Review Meeting: 934-974 Marchwood House**

Tuesday 9 March 2021  
Zoom Video Conference

**Panel**

Peter Bishop (chair)  
Irfan Alam  
Marie Burns  
Thomas Bryans  
Michael Popper

**Attendees**

Paul Baxter	Watford Borough Council
Andrew Clarke	Watford Borough Council
Phil Dodshon	Watford Borough Council
Sian Finney-MacDonald	Watford Borough Council
Tom Bolton	Frame Projects
Miranda Kimball	Frame Projects
Penny Nakan	Frame Projects

**Apologies / report copied to**

Louise Barrett	Watford Borough Council
Ben Martin	Watford Borough Council

**Confidentiality**

This is a pre-application review, and therefore confidential. As a public organisation Watford Borough Council is subject to the Freedom of Information Act (FOI), and in the case of an FOI request may be obliged to release project information submitted for review.

# CONFIDENTIAL

## 1. Project name and site address

Garston Garage, 934-974 Marchwood House, St Albans Road, Watford, WD25 9NN

## 2. Presenting team

David Chalmers	Fairview New Homes
Mark Jackson	Fairview New Homes
Andy Reid	Fairview New Homes
Graig Rodgers	Fairview New Homes
Jason Rudolph	Rolfe Judd Architects
Angus Drummond	Rolfe Judd Architects
Alejandro Penalver	Rolfe Judd Architects
Mike Martyn	Cameo Landscape Architects
Heidi von Kotze	Cameo Landscape Architects
Paul White	Turley
David Chalmers	Turley
Colin Morrison	Turley

## 3. Planning authority briefing

The site, which houses the former Garston Bus Garage, is broadly rectangular, 1.19 hectares in area and 5 km from Watford Town Centre. A locally listed administration building fronts directly onto St Albans Road, and a substantial industrial building occupies the majority of the rear of the site.

The site is bordered by Garston Park on one side and, to the rear and south, by the gardens of two-storey post-war residential properties on Codicote Drive and Felden Close. There are two existing access points directly onto St. Albans Road. The southernmost access abuts the two-storey locally listed Garston Park shopping parade. There is variety in building types, forms, sizes, and functions in the area, but the broadly consistent scale and heights of no more than three storeys contribute to a townscape of a suburban scale and appearance.

This proposal is the second proposal for this site by the same applicant. Planning permission was refused for the first proposal for 165 flats in two buildings of up to five storeys, in December 2019, and dismissed at appeal by the Planning Inspectorate in October 2020. The inspector considered the character and appearance of the first proposal unacceptable. The principle of residential redevelopment is accepted. While the appeal decision raised concerns with design and layout, the following matters were agreed upon:

- The site is suited for higher density development.
- The loss of the locally listed administration building is accepted, being regarded as having low interest in architectural and historic terms.
- Proposed buildings would be sited at a greater distance from neighbouring properties than the present industrial structure and, while still visible, would be less obtrusive for residents – particular properties along Codicote Drive.



# CONFIDENTIAL

- Parking provision meets the Council's maximum standards, and no highway safety or on-street parking concerns arise.

The revised proposals seek to respond to the concerns raised by the previous application and proposes 127 residential units in a mixture of one, two and three-bed units. Officers asked for the panel's views in particular on:

- The layout of the site, with a single vehicular cul-de-sac.
- The legibility of the link to Garston Park.
- Car dominance of the public realm, with encroachment of the car parking and turning heads into the main central amenity space.
- Quality of amenity spaces, and the space adjacent to the eastern boundary.
- The extent of non-active frontage and lack of entrances at ground floor level.
- The lack of duplex units or family houses as part of the overall mix.
- The low proportion – 13 per cent – of three-bedroom dwellings.
- Quantity of car parking and cycle storage.

## 4. Design Review Panel's views

### *Summary*

The panel considers that the density of the proposed development is at the limit of the site capacity, and an improved site layout is required to avoid a detrimental impact on the quality of public and private space at ground floor level. Improvements to the overall site layout and massing are needed to reduce the area of car parking and hard landscaping, which dominates the public realm. Closure of the southern access point should be challenged to ensure all layout options are explored. These should include orientations that reduce the risk of solar gain in east-west aligned blocks. Amenity spaces should be designed for different age groups, a lighting strategy produced and a clearer relationship created with Garston Park. The panel also considers that the removal of the locally listed building can only be justified by construction of higher quality buildings in its place, and that this is not yet the case. The architectural quality of the scheme is ambitious, but could be further improved by introducing greater clarity, including simplifying materials and elevations, and integrating balconies. Sustainability ambitions should be raised, and the installation of gas boilers avoided. These comments are expanded below.

### *Massing and layout*

- The panel suggests that the proposed massing is at the limit of what is appropriate for the scale of the site. A better balance should be sought between overall development quantum and a site layout that best serves the development.
- The site is unrestricted, and therefore offers the freedom to design optimum massing and layout, with a low proportion of single aspect units. The panel feels the applicant should re-visit the massing, layout, and aspect to deliver the highest possible quality development, which is required to justify the demolition of the locally listed building.



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- An improved site layout is also needed to reduce the high proportion of single aspect units. The panel notes that refuse and cycle storage are the only parts of the blocks with triple aspect, and asks that the quality of internal layouts is improved.
- The scale of Blocks E, F, and G feels inappropriate in relation to the domestic scale of the homes on Codicote Drive, and the panel suggests this should be revisited.
- The panel questions whether the alleyways running between Blocks E, F and G will create spaces that are pleasant to use.

## *Architecture*

- The panel challenges the planning inspector's decision that the architectural quality of the locally listed administrative building is low, and that its demolition is therefore justified. It considers that its demolition can only be justified if it is replaced by a building of higher quality. Changes to the design approach are needed to ensure the current proposals reach the level of quality this requires.
- The panel considers that the proposed architectural approach would benefit from greater clarity and simplicity.
- In particular, it suggests the design team reduces the number of different brick tones used and employs highlighted bricks more sparingly.
- The balconies feel separate from the blocks they are attached to. The panel challenges the design team to find a solution that integrates the balconies with the base architecture of the blocks.
- The panel also considers that balconies overlooking the busy St. Albans Road may not provide a high enough quality of amenity space.
- The panel suggests that the projecting bays on the western frontage appear awkward, and that this façade should be simplified.

## *Landscaping and amenity space*

- The panel commends the work carried out to develop the landscaping strategy for the site. However, it is concerned that the quality of public and private space is adversely affected by the density of the proposed development.
- Hard landscaping and car parking provision is too dominant. The number of parking spaces should be reduced to improve the quality of place, even if this requires a reduction in density.



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- The panel also encourages the design team to consider the provision of a wider range of amenity space types to ensure there are spaces designed for all ages – especially for younger and for older children.
- The panel suggests that the nature of the central courtyard space is ambiguous. Rather than a semi-public space, residents may prefer a more generous, secure external space that allows children to play. The best quality secure spaces on the site are currently the gardens and allotments to the rear of Blocks E, F and G, which are only accessible to some residents.
- A resolved lighting strategy will be important to help improve the security and overall attractiveness of the development, and to contribute to creating a sense of place.

### *Connections*

- The panel encourages the applicant to work with Watford Borough Council and Hertfordshire County Council, as highways authority, to challenge the requirement to close the southern egress point. It considers that removing the constraints created by a single access point could enable the design of a more successful site layout.
- The proximity of Garston Park is a major asset for the site, and further thought should be given to the way the development relates to it. The link to the park could be more legible, and perhaps simplified with a single access route along the northern edge of the site, without the need for a second, diagonal link through the main courtyard space.

### *Sustainability*

- The panel asks that the sustainability strategy for the development is revisited. There is scope for it to be more ambitious and, in particular, to identify alternative approaches that avoid the installation of gas boilers.
- The panel also suggests that the orientation of blocks should be reconsidered, to explore options that avoid east-west aspects, and therefore reduce the risk of solar gain.

### *Next Steps*

- The panel is available to review this scheme again, once the design team has had the opportunity to address the issues it has raised.





**Watford Place Shaping Review Panel**

**Report of Chair's Review Meeting: 934-974 Marchwood House**

Tuesday 11 May 2021  
Zoom Video Conference

**Panel**

Peter Bishop (chair)  
Irfan Alam

**Attendees**

Paul Baxter	Watford Borough Council
Andrew Clarke	Watford Borough Council
Sian Finney-MacDonald	Watford Borough Council
Ben Martin	Watford Borough Council
Linda Bishop	Watford Borough Council
Carmel Huntley	Watford Borough Council
Tracey Lynch	Watford Borough Council
Tom Bolton	Frame Projects
Penny Nakan	Frame Projects

**Apologies / report copied to**

Louise Barrett                      Watford Borough Council

**Confidentiality**

This is a pre-application review, and therefore confidential. As a public organisation Watford Borough Council is subject to the Freedom of Information Act (FOI), and in the case of an FOI request may be obliged to release project information submitted for review.

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## 1. Project name and site address

Garston Garage, 934-974 Marchwood House, St Albans Road, Watford, WD25 9NN

## 2. Presenting team

David Chalmers	Fairview New Homes
Andy Reid	Fairview New Homes
Graig Rodgers	Fairview New Homes
Jason Rudolph	Rolfe Judd Architects
Angus Drummond	Rolfe Judd Architects
Alejandro Penalver	Rolfe Judd Architects
Mike Martyn	Cameo Landscape Architects
Heidi von Kotze	Cameo Landscape Architects

## 3. Planning authority briefing

The site, which houses the former Garston Bus Garage, is broadly rectangular, 1.19 hectares in area and 5 km from Watford Town Centre. A locally listed administration building fronts directly onto St Albans Road, and a substantial industrial building occupies the majority of the rear of the site. The site is bordered by Garston Park on one side and, to the rear and south, by the gardens of two-storey post-war residential properties on Codicote Drive and Felden Close.

This proposal is the second proposal for this site by the same applicant. Planning permission was refused for the first proposal for 165 flats in two buildings of up to five storeys, in December 2019, and dismissed at appeal by the Planning Inspectorate in October 2020. The inspector considered the character and appearance of the first proposal unacceptable. The principle of residential redevelopment is accepted.

The revised proposals seek to respond to the concerns raised by the previous application and proposes 127 residential units in a mixture of one, two and three-bed units. The proposals were reviewed by the Watford Place Shaping Panel in March 2021. Since then, amendments have been made to move building footprints further from neighbouring properties; to increase the percentage of dual aspect units to 66 per cent all units have been designed to meet or exceed Technical Housing Standards; to refine the architecture of Blocks E-G; to reduce onsite parking; to enhance the entrance to Garston Park; to refine internal amenity areas; and to alter the energy strategy to use central gas-fired boilers and Air Source Heat Pumps, with roof-mounted photovoltaic cells. A planning application has now been submitted.

Watford officers asked the panel to consider whether these improvements address the panels concerns and, in particular, for its views on dominance of hard landscaping, progress on sustainability, the layout of Blocks C and D, and whether the frontage on St Albans Road is of sufficient quality to justify demolition of the locally listed building. Officers also asked for views on a southern access loop, on which they are confirmation of the highways authority's position.



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## 4. Design Review Panel's views

### *Summary*

The panel considers that the proposals have improved in several ways since the previous review, but still considers that an improved site layout and design approach would deliver a higher quality development. The quantum of development on the site results in a compromised site layout, and undermines the quality of space at ground floor level. The panel is also concerned that the deep spaces and blank walls between Blocks D-G will create unwelcoming spaces. The simplified material range and palette is a success, as is the rationalised elevation of Blocks A and B on St. Alban's Road. More thought should be given to integrating balconies with the buildings, and to refining the height of parapets. The panel asks that the width of the access road is reduced, to create a more welcoming pedestrian route to Garston Park, ideally separated from vehicles with greenery. A clearer rationale is needed for the design of the landscaped area fronting St. Albans Road. Improvement to the energy strategy are very welcome, and anything that can be achieved to push this even further will be beneficial. These comments are expanded below.

### *Site plan*

- The panel's comments about the density of the proposed development, made at the previous review, remain valid: that a better balance should be sought between quantum of residential units and a site layout that best serves the development. Alterations, including the removal of four parking spaces, are beneficial. However, despite alterations to the design the footprint of the blocks remain fundamentally the same, and the amount of hard-standing has not changed significantly. Therefore the panel still considers that the site layout is constrained and that this has a detrimental impact on the quality of space at ground floor level.
- The panel accepts assurances that Hertfordshire County Council has categorically rejected the option of a southern site access point, but still considers that negotiation would be possible, and valuable. An access loop would provide much more flexibility for the location of blocks, and an improved layout could be achieved.

### *Architecture*

- The panel welcomes the proposed changes in materials and palette across the scheme, which are positive moves that help to simplify the architectural approach.
- It also considers that moving the balconies around to the sides of the St. Albans Road Blocks A and B is a successful rationalisation of the design. The development's main public elevation now appears simpler and better detailed.



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- However, the panel feels the design of the balcony units themselves could be improved. They still seem separate from the rest of the building. The team should explore whether their design can be improved to better integrate them.
- The panel expressed conflicting views on whether further design changes would benefit the St Alban's Road elevation. The flanks at either end of the façade could be considered to narrow, with excessively thin glazing strips. There could be scope to introduce greater generosity into these areas.
- On the other hand, the narrow windows on the end blocks could be considered to give the elevation an improved sense of proportion, and to make references to arch of Garston Bus Garage. It is for the design team to decide how it responds to these differing opinions.
- The panel suggests that the parapets have increased in size since the designs seem at the previous review, and appear a little top heavy. The team should look again at parapets to explore whether their design can be refined.
- The panel still has concerns about the gaps between Blocks D-G. The blocks are significantly larger than the two-storey houses suggested as a precedent, which also have windows in their flank walls and pitched roofs, helping to reduce the scale where buildings meet. The panel feels that the spaces between these blocks will feel canyon-like and unwelcoming, and asks that more is done to address the issue.

### *Landscaping*

- The panel considers that more work is needed to develop designs for the landscaping on the St. Albans Road site frontage. It is not clear whether this is a buffer area or a space that people will walk through. A stronger rationale is needed for the way the space will be used, which should inform the planting approach and a decision on whether to include low railings or a hedge along the boundary.
- While the panel understands the inclusion of fences around the central green space, it considers gates should definitely be avoided.

### *Entrance road*

- The increased emphasis on access to Garston Park is a positive change to the proposals. However, the panel considers the entrance road should be narrowed as far as possible, to reduce vehicle dominance and create more space for pedestrians. The carriageway width could be reduced along its full length by relocating spaces from the northern to the southern stretch of access road. This would also provide greater flexibility to improve the landscaping in the area where the public will walk to access the park, where it matters most.
- The panel asks the design to rethink this aspect of the scheme. They should



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consider increasing the width of the footpath leading to the park, and potentially include greenery to separate the pedestrian route from the road alongside. The aim should be to create a stronger, more legible connection with the Garston Park.

- The entrance to the park itself could also be repositioned if needed, to help ensure the park entrance is as welcoming and accessible as possible.

### *Sustainability*

- The panel is pleased to see that its request for a more ambitious sustainability strategy have been on board, and that energy use not only aligns better with current requirements, but are future-proofed. It encourages the design team to look for further scope to reduce carbon further, and to take any opportunity it can identify.

### *Next Steps*

As the planning application is now live, the panel is confident that Watford Borough Council can decide whether the proposals successfully address its comments.



## Agenda Item 6

<b>Committee Date</b>	Tuesday 7 September 2021
<b>Application Reference</b>	21/00934/VARM
<b>Site Address</b>	Hannay House, 37-39 Clarendon Road, Watford WD17 1JA
<b>Proposal</b>	Variation of conditions 2, 21, 22 and 23 of planning permission references 17/00470/FULM and 21/00933/NONMAT
<b>Applicant</b>	Regal Clarendon Ltd
<b>Agent</b>	Regal Clarendon Ltd
<b>Type of Application</b>	Variation of Condition
<b>Reason for Committee Item</b>	Major Development
<b>Target Decision Date</b>	Friday 10 <sup>th</sup> September 2021 (extended by agreement)
<b>Statutory Publicity</b>	Site Notice and Neighbour Letters
<b>Case Officer</b>	Helen Harris; helen.harris@watford.gov.uk
<b>Ward</b>	Central

### 1. Recommendation

- 1.1 That planning permission be granted, subject to a Deed of Variation executed under s106 of the Town and Country Planning Act 1990 (as amended), subject to conditions as set out in Section 8 of this report.

### 2. Site and Surroundings

- 2.1 The site is located on the western side of Clarendon Road, close to the junction with Beechen Grove. It is rectangular in shape and has an area of 0.40 hectare. It adjoins Jury's Inn to the south and has a rear boundary that adjoins Beechen Grove. It comprises of a 4-storey office block on the frontage (Hannay House) which dates from the 1960s. To the rear is a second, 3-storey office block (Millbuck House) which has been vacant since 1997.
- 2.2 The site is located in the Town Centre SPA in the Core Strategy and within an allocated employment area (E7a) in the Watford District Plan 2000. There are parking restrictions, including a Residential Control Parking Zone along the roads within the vicinity of the site. The site is not located within a conservation area and there are no listed or locally listed buildings within, or adjoining, the site.

### **3. Summary of the Proposal**

#### **3.1 Proposal**

3.2 To vary the drawings approved by application reference 17/00470/FULM and the subsequent amendments approved under reference 21/00933/NONMAT to increase the number of units from 154 to 168 within the approved envelope of the existing building with associated elevational changes. The approved mix of accommodation and affordable housing contribution would be amended.

#### **3.3 Planning Balance and Conclusion**

3.4 The change to the mix of accommodation, with an increased provision of one and three bedroom units from that originally consented is considered acceptable in this location. The proposed changes to the approved building are relatively minor and are required predominantly to comply with certain specific Building Regulations and Fire Safety Regulations. The proposal would not appear externally to be significantly different than the consented scheme.

3.5 The cycling and refuse stores are of a sufficient size to accommodate the increase in cycle parking and refuse requirements.

3.6 There would be no additional detriment to the light, outlook or privacy of neighbouring properties incurred relative to the consented scheme.

3.7 The uplift in the affordable housing contribution of £150,000, from £1.65M to £1.8M, subject to indexation, for the additional 14no. units is considered the maximum reasonable contribution that the scheme can viably make towards the provision of affordable housing off-site.

3.8 Taking the above into account, there are considered to be no adverse effects that outweigh the benefits of the proposal and therefore it is recommended that the application should be approved.

### **4. Relevant Policies**

4.1 Members should refer to the background papers attached to the agenda. These highlight the policy framework under which this application is determined. Specific policy considerations with regard to this particular application are detailed in Section 6, below.

4.2 Paragraph 11 d) of the NPPF 2021 establishes the 'presumption in favour of sustainable development' and the principles of the 'tilted balance' that apply where a local planning authority cannot demonstrate a 5 year housing supply

or have failed to deliver at least 75% of their housing requirement as part of the Housing Delivery Test. Where the tilted balance applies, decision makers should grant permission unless NPPF policies on protected areas or assets of particular importance provide a clear reason for refusing development or, any adverse impacts of granting permission would significantly and demonstrably outweigh the benefits, assessed against NPPF policies taken as a whole. The tilted balance has the effect of shifting the weight in the planning balance away from local policies and towards the NPPF.

- 4.3 The Council cannot currently demonstrate a 5 year supply of housing and scored below 75% in the most recent Housing Delivery Test results and therefore the 'tilted balance' applies to the determination of this planning application.

## **5. Relevant Planning History**

- 5.1 The application site has the following planning history:

- 5.2 17/00470/FULM – Proposed mixed use development comprising up to 11,180 sqm of Class B1(a) office space on 8 floors, 154 residential units on 23 floors, up to 496 sqm of Class A3 café/restaurant space on the ground and 9th floors, 1st floor gym, basement car and cycle parking, access, landscaped rooftop amenity space and associated works. Conditional Planning Permission was granted on 1<sup>st</sup> May 2018, subject to a signed s.106 legal agreement. On 29<sup>th</sup> July 2020, a deed of variation to the Section 106 was approved at Committee. The deed of variation sought to fix the affordable housing contribution and remove the review mechanism.

- 5.3 18/00894/NONMAT - Section 96a application for design revisions including:
- Omission of one level of basement and increase in size of the remaining two upper level basement floors;
  - Reduction in car parking from 200 to 180 spaces;
  - Relocation of substation from basement to ground level;
  - Adjustment of floor to ceiling heights within both the office and residential floorspace;
  - Overall increase in building height by 1.6m (above the approved building parameters);
  - Internal reconfiguration of mezzanine level and back of house plant/servicing areas; and
  - Increase in ground floor reception area and relocation of café.

Approved on 3<sup>rd</sup> December 2018.

- 5.4 20/00600/NONMAT - Section 96a application for internal reconfiguration of the ground floor office entrance, increase in the level of ceiling height at mezzanine level and creation of 1,065sqm additional office space.

Approved on 16<sup>th</sup> June 2020.

- 5.5 21/00933/NONMAT – Section 96a application for the description of development. (Proposed mixed use development comprising Class E office space, Class C3 residential units together with Class E cafe/restaurant, ancillary gym, basement car and cycle parking, access, landscaped rooftop amenity space and associated works.)

Approved on 2<sup>nd</sup> July 2021.

- 5.6 These three subsequent Section 96a amendment applications are to be read alongside the original 2018 planning permission which collectively form the implemented set of planning approvals for the site.
- 5.7 Following the discharge of all pre-commencement conditions in April 2021, a Certificate of Lawfulness Application was granted on 7<sup>th</sup> April 2021 (ref: 21/00520/LDC) confirming the lawful implementation of planning permission 17/00470/FULM. This means that notwithstanding the outcome of the current application that the original permission granted in 2018 remains extant.

## **6. Main Considerations**

- 6.1 This variation of condition application only considers matters where this proposal differs from the consented scheme which remains extant. The principle of the development, the scale of the building, the impact upon heritage assets, the impact upon surrounding properties and environmental considerations are unchanged.
- 6.2 The main issues to be considered in the determination of this application are:
- (a) Design;
  - (b) Housing Mix;
  - (c) Quality of the Proposed Residential Accommodation;
  - (d) Affordable Housing Provision; and
  - (e) Servicing and Parking
- 6.3 (a) Design
- 6.4 Relative to the consented proposal, there would be minor changes to the elevations. These changes are required to ensure compliance (and in some

instances, exceedance) with the necessary Building Control and Fire Safety Regulations.

- 6.5 The proposed design changes include the following:
- Relocation of the glass balustrading along the inside wall in order to provide the impression of a Juliette balcony to the residential flats;
  - Insertion of new structural openings to allow for increased daylight provision to some of the consented units which do not benefit from natural outlook;
  - Substitution of glazed spandrel panelling to PPC aluminium to achieve fire safety compliance;
  - Enhancement to the fenestration and rhythm of the façade to align with the new internal layouts and structural column design.
- 6.6 The application also proposes a number of design enhancements within the residential accommodation to improve the quality of amenity and buildability of the residential flats. These are as follows:
- Improved internal configuration at first floor level by rationalising the plant space to ground / basement level and omitting the mezzanine floor entirely to improve the quality of natural light within the two newly created residential units at level 1;
  - Omission of the duplex units within the penthouse flats to provide three additional flats;
  - Increase in ground floor to ceiling heights to allow for sufficient ceiling mounted services; and
  - Reduction in the storey height of each residential floor by 100mm (from 3.1m to 3.0m) and lowering the crown by one metre to ensure a more optimum wind brace design.
- 6.7 The proposed change to the positioning of the glass balustrades along the internal face of the wall will not significantly alter the external appearance of the building and the full-height sliding door arrangement to the façade as originally consented will remain unchanged. The rhythm of the façade has been updated to align with the structural column design and the new internal layouts of the flats. The internal glazing to the winter gardens has been omitted to allow for better functionality of the living room spaces and to lessen the 'glass box' overheating effect to the internal accommodation resulting from excessive solar gain. The fenestration proposed would continue to function as a Juliette balcony in the living room.
- 6.8 The separation distances from surrounding buildings is unchanged, which remains compliant with Watford's Residential Design Guide.

6.9 (b) Housing Mix

6.10 The proposed development is in a sustainable town centre location close to public transport facilities and is not on land at risk of flooding or within designated employment land, open space or land with high biodiversity, landscape or cultural heritage significance. Therefore, the proposal is consistent with Policy HS1 of the Core Strategy. The proposal would contribute towards meeting the Borough's housing target, which should be afforded considerable weight in consideration of the application given the Council's current position in terms of housing delivery

6.11 The consented mix of accommodation approved comprises 54 x studios (35%), 48 x 1 bed (31%), and 52 x 2 bed (34%) units. The proposed mix of accommodation comprises 55 x studios (33%), 54 x 1 bed (32%), 50 x 2 bed (30%) units and 9 x 3-bed (5%) units.

6.12 This revised housing mix would allow for the addition of 9no. 3-bedroom flats to the scheme. The introduction of 3-bedroom family-sized homes is welcomed and responds positively to the emerging draft Local Plan policy which, when adopted, will set a strategic target that 20% of all new homes should be family sized (3-bedroom+). The proposed housing mix is considered acceptable given the accessible, relatively central location of this development, where higher density development is considered acceptable.

6.13 (c) Quality of the Proposed Residential Accommodation

6.14 Within the main tower, consolidation of the floor-to-ceiling heights have allowed for two additional storeys to be created without any significant increase to the overall height of the tower, with six flats on each floor (total addition of 6no. 1-bed, 2-person sized flats and 6no. 2-bed, 4-person sized flats) which are arranged around the main lift/stair core. These will face in all directions and all will be either south facing or dual aspect with at least one elevation facing west, south or east. This will ensure the vast majority of the flats will have good levels of outlook, daylight, sunlight and privacy.

6.15 The club room has been omitted at the first floor and all of the residential amenities are now shown to be consolidated at the ground floor. Two flats have been added at first floor. One would be a 2-bed, 3-person sized flat and one would be a 1-bed, 2-person sized flat and both would be afforded good south-facing aspect.

6.16 All of the proposed residential units will meet or exceed the Nationally Described Space Standards for new residential dwellings and will have good levels of natural light, outlook and privacy. As before, conditions for appropriate glazing and mechanical air supply/extract system will ensure suitable internal noise

levels and ventilation. Overall, the quality of the proposed accommodation is considered to be acceptable.

6.17 (d) Affordable Housing Provision

- 6.18 Policy HS3 of the Core Strategy states that a rate of 35% affordable housing will be sought on major applications of 10 residential units and above and only in exceptional circumstances will the council consider a lower level of affordable housing provision, where the developer can demonstrate exceptional planning, or other constraints on the development of the site through the submission of a development viability appraisal. In this case, as part of the original consent the Council agreed that no on-site affordable housing would be provided and instead the development would provide a financial contribution in lieu towards affordable housing off-site. The level of contribution to be provided was originally subject to a review mechanism to be implemented towards the end of the project. However, in July 2020 the Development Management Committee approved a deed of variation to the s106 agreement to remove the review mechanism and fix the contribution at £1.65M. This contribution was established through a viability appraisal of the original consented scheme, independently verified by the Council's own advisors, and was agreed to be the maximum reasonable amount that could be secured from the original scheme.
- 6.19 The current proposals would see an uplift in the number of homes (and consequently residential floorspace) to be delivered through the scheme and therefore the impact of this change on the viability of the scheme needs to be considered. The applicant has submitted an updated viability appraisal to assess the impact of the proposed changes on the viability of the development.
- 6.20 The Council has appointed Aspinall Verdi to independently review the applicant's viability appraisal submitted as part of this planning application. Their assessment shows that the proposed development, including the proposed changes, generates a deficit of £12M and therefore concludes the scheme could not viably make a financial contribution towards the provision of policy compliant on site affordable housing. Notwithstanding the position on viability, a contribution of £1.65M has already been agreed reflecting the critical need for affordable housing in Watford and recognising this the applicant has offered a pro-rata increase on the agreed contribution of £150,000 based on the increase of 14no. additional residential units. This pro-rata increase would increase the base contribution, before indexation is applied, from £1.65M to £1.8m. When current indexation is applied to the original £1.65M contribution the present day value of the contribution is circa £2M.

- 6.21 (e) Servicing and Parking
- 6.22 Relocation of the ramp along the northern elevation, together with the increase in building footprint towards Clarendon Road has provided for an improved basement layout, allowing for the increase of up to 30no. vehicular parking spaces at lower ground floor level.
- 6.23 The applicant has once again agreed that the development be excluded from the CPZ in order to ensure future residents are not entitled to parking permits and to avoid the possibility of overspill parking from the development. This can continue to be secured by a s.106 planning obligation.
- 6.24 The consented permission had 154no. cycle spaces (based on a ratio of 1no. space per unit). This ratio is maintained in this application with an increase in cycle parking provision proposed to 168no. cycle storage spaces (to reflect the proposed uplift in the number of residential units). The commercial cycle store is split from the residential store and the proposed application provides 96no. long-term and 25no. short-term cycle spaces. This compares with the 94no. long-term and 24no. short-term commercial cycle provision under the consented permission. This complies with cycle space standards.
- 6.25 With regards to the bin store, on the approved ground floor plan (drawing ref: 10370\_PL\_100 Rev E), a 3-fold arrangement of bins was shown which was considered acceptable. The proposed bin storage provision follows the same principles established under the original consent which seeks to provide the total number of bins required by the council within a similar sized storage area (drawing ref: 1152-PL-AA00). This area is sufficient to accommodate the refuse and recycling requirements of the additional proposed units.
- 6.26 A 24-hour concierge is proposed to be provided at this development, together with a managed strategy to rotate the bins when they are filled to capacity until collection day, so that residents are always able to access empty or partially empty bins when required (to prevent any excessive piling up of waste).
- 6.27 It is considered that the refuse and cycle stores have adequate capacity to serve the proposed increased number of units.

## **7. Consultation Responses Received**

### **7.1 Statutory Consultees and Other Organisations**

None consulted.

## 7.2 Internal Consultees

Name	Officer's Comment
Waste and Recycling	Object on the basis of the bin layout.  Officers note that waste and recycling team did not issue any comment to the original scheme, which proposed a similar arrangement. The capacity of the bin store is considered sufficient to accommodate the additional units.  Please refer to 6.25 and 6.26 of this report for response to this objection.

## 7.3 Interested Parties

Letters were sent to 162no. properties in the surrounding area. No responses have been received.

## 8. Recommendation

That, pursuant to a Deed of Variation under s.106 of the Town and Country Planning Act 1990 having been completed to secure the following Heads of Terms, planning permission be granted subject to the conditions listed below:

### Deed of Variation Heads of Terms

- i) To secure a total financial contribution of £2,008,241 in lieu of on-site affordable housing. (i.e. uplift from £1,650,000 to £1, 858,241 allowing for indexation, plus an uplift of £150,000)
- ii) To secure a financial payment to the Council of £2,000 towards the variation of the Borough of Watford (Watford Central Area and West Watford Area)(Controlled Parking Zones)(Consolidation) Order 2010 to exclude the site from the controlled parking zone, thereby preventing residents' parking permits being issued to this site.

## Conditions

1. The development to which this permission relates shall be begun within a period of three years commencing on the date of this permission.

Reason: To comply with the requirements of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved drawings:

1152-PL-AA90 SITE LOCATION PLAN

10370-PL\_104\_-\_3RD-6TH\_FLOOR\_PLAN10370-PL\_105\_-\_7TH\_FLOOR\_PLAN

10370-PL\_106\_-\_8TH\_FLOOR\_PLAN

10370-PL\_107\_-\_9TH\_FLOOR\_PLAN

10370-PL\_108\_-\_10TH-19TH\_FLOOR\_PLAN

10370-PL\_120\_-\_BASEMENT\_LEVEL\_1-681521(1)

10370-PL\_121\_-\_BASEMENT\_LEVEL\_2-681519(1)

10370\_PL\_130\_F\_-\_SOUTH\_ELEVATION

10370\_PL\_131\_E\_-\_WEST\_ELEVATION

10370\_PL\_132\_F\_-\_EAST\_ELEVATION

10370\_PL\_133\_F\_-\_NORTH\_ELEVATION

10370\_PL\_134\_E\_-\_SECTION\_ELEVATIONS

10370PL\_100\_E\_GROUND\_FLOOR\_PLAN-856430(1)

10370PL\_101\_D\_MEZZANINE\_FLOOR-856431(1)

10370PL\_102\_E\_1ST\_FLOOR\_PLAN-856432(1)

10370PL\_103\_E\_2ND\_FLOOR\_PLAN-856433(1)

1152-PL-AA00 - GROUND FLOOR PLAN

1152-PL-AA01 - 1ST FLOOR PLAN

1152-PL-AA02 - 2ND FLOOR PLAN

1152-PL-AA03 - 3RD FLOOR RESIDENTIAL

1152-PL-AA04 - 4TH FLOOR RESIDENTIAL 3<sup>RD</sup> FLOOR OFFICE

1152-PL-AA05 - 5TH FLOOR RESIDENTIAL 4<sup>TH</sup> FLOOR OFFICE

1152-PL-AA06 - 6TH FLOOR RESIDENTIAL 5<sup>TH</sup> FLOOR OFFICE

1152-PL-AA07 - 7TH FLOOR RESIDENTIAL  
1152-PL-AA08 - 8TH FLOOR RESIDENTIAL 6<sup>TH</sup> FLOOR OFFICE  
1152-PL-AA09 - 9TH FLOOR RESIDENTIAL 7<sup>TH</sup> FLOOR OFFICE  
1152-PL-AA10 - 10TH FLOOR RESIDENTIAL 8<sup>TH</sup> FLOOR OFFICE  
1152-PL-AA11 - 11TH FLOOR RESIDENTIAL 9<sup>TH</sup> FLOOR OFFICE  
1152-PL-AA12 - 12TH-21ST FLOOR RESIDENTIAL  
  
1152-PL-AA13 - 22ND-24TH FLOOR RESIDENTIAL  
1152-PL-AA14 - 25TH FLOOR RESIDENTIAL  
1152-PL-AA20 - BASEMENT LEVEL -1 FLOOR PLAN  
1152-PL-AA21 - BASEMENT LEVEL -2 FLOOR PLAN  
1152-PL-AA30 - SOUTH ELEVATION  
1152-PL-AA300 - RESIDENTIAL - GIA UNIT AREAS ACCOMMODATION  
SCHEDULE  
1152-PL-AA301 - OFFICE - AREAS  
1152-PL-AA31 - WEST ELEVATION  
1152-PL-AA32 - EAST ELEVATION  
1152-PL-AA33 - NORTH ELEVATION  
1152-PL-AA34 - SECTION ELEVATIONS  
1152-PL-AA502 - RESIDENTIAL FACADE  
1152-PL-AA503 - OFFICE FACADE  
1152-PL-AA504 - CAFE\_OFFICE FACADE  
1152-PL-AA91 RED LINE PLAN  
PL\_501 – WINDOW CLEANING STRATEGY  
DAS ADDENDUM (DATED JUNE 2021)

Reason: For the avoidance of doubt and in the interests of proper planning.

3. The development permitted by this planning permission shall be carried out in accordance with the approved Drainage Strategy Report by SLR Consulting (ref. 402.06661.00004 Version 2.0, dated July 2017) approved as part of planning permission reference: 17/00470/FULM and the following mitigation measures detailed within the FRA:
  - i) Limiting the surface water run-off rates to maximum of 54.3l/s for the 1in 100 year rainfall event with discharge into Thames Surface Water sewer.

- ii) Provide attenuation to ensure no increase in surface water run-off volumes for all rainfall events up to and including the 1 in 100 year + climate change event. iii) Implementing the appropriate drainage strategy using appropriate SuDS measures as indicated on drawing no. 1620002979-RAM-XX-DRC-00100 Rev P2.

The mitigation measures shall be fully implemented prior to occupation and subsequently in accordance with the timing/phasing arrangements embodied within the scheme, or within any other period as may subsequently be agreed, in writing, by the local planning authority.

Reason: To prevent flooding by ensuring the satisfactory storage of and disposal of surface water from the site.

4. The development permitted by this planning permission shall be carried out in full accordance with the approved Drainage Layout 9ref: 140682/2000\_rev D, dated 15.02.2021, as approved under planning reference: 20/01485/DISCON.

Reason: To reduce the risk of flooding to the proposed development and future occupants.

5. No external facing materials shall be installed on the building until full details and samples of all the materials to be used for the external surfaces of the building, including the plant enclosures at roof level, have been submitted to and approved in writing by the Local Planning Authority.

Reason: In the interests of the visual appearance of the building and the character and appearance of the area, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

6. No external facing materials shall be installed on the building until a noise mitigation scheme for all the proposed residential dwellings on the 1 st-6th floors with windows facing Beechen Grove, based upon the recommendations of the Environmental Noise Assessment by SLR dated March 2017 (Ref. No. 403.06661.00003, Version Final), has been submitted to and approved by the Local Planning Authority. The scheme shall include the details and specifications of the sound reduction performance of all glazed and non-glazed elements of the building facades. No dwelling shall be occupied until the approved mitigation measures have been installed in full, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure good indoor ambient noise levels are achieved in accordance with BS 8233:2014 for the future occupiers of the dwellings.

7. No external facing materials shall be installed on the building until the specification of a mechanical air supply/extract system for each of the residential dwellings on the 1st-6th floors with windows facing Beechen Grove has been submitted to and approved in writing by the Local Planning Authority. The system must be capable of providing background and rapid ventilation for cooling with the windows of the dwellings being closed. The system must not compromise the sound insulation of the façades. Details of the siting of any air intake; extraction units; generators and other mechanical equipment serving this system that are likely to give rise to noise should be submitted, along with details of noise attenuation measures to be incorporated to ensure these units do not give rise to a noise nuisance. No dwelling shall be occupied until the approved mitigation measures have been installed in full, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure good indoor ambient noise levels are achieved in accordance with BS 8233:2014 for the future occupiers of the dwellings.

8. No external facing materials shall be installed on the building until the specification of a mechanical air supply/extract system for each of the residential dwellings on the 8 th-11th floors with windows on the south elevation facing Jury's Inn has been submitted to and approved in writing by the Local Planning Authority. The system must be capable of providing background and rapid ventilation for cooling with the windows of the dwellings being closed and be able to filter out cooking odours. The system must not compromise the sound insulation of the façades. Details of the siting of any air intake; extraction units; generators and other mechanical equipment serving this system that are likely to give rise to noise should be submitted, along with details of noise attenuation measures to be incorporated to ensure these units do not give rise to a noise nuisance. No dwelling shall be occupied until the approved mitigation measures have been installed in full, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To prevent ingress of cooking odours to the flats from the kitchen extract of Jury's Inn adjoining the site, in the interests of the future occupiers of the dwellings.

9. No impact piling shall take place until a piling method statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the

programme for the works) has been submitted to and approved in writing by the Local Planning Authority. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

Reason: The proposed works will be in close proximity to underground sewerage utility infrastructure. Piling has the potential to impact on local underground sewerage utility infrastructure.

10. No part of the development shall be occupied until a detailed hard landscaping scheme for the site, including details of the roof gardens, site boundary treatments and external lighting, has been submitted to and approved in writing by the Local Planning Authority, and the works have been carried out in accordance with the approved details. The detailed scheme shall be based upon the Landscape Strategy contained within the Design and Access Statement dated 3rd April 2017 by PRC Architecture and Planning approved as part of planning permission reference: 17/00470/FULM.

Reason: In the interests of the visual appearance of the site and the wider area, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

11. No part of the development shall be occupied until a detailed soft landscaping scheme for the site, including details of the roof gardens and appropriate irrigation systems, and a landscape management and maintenance plan, has been submitted to and approved in writing by the Local Planning Authority. The detailed scheme shall be based upon the Landscape Strategy contained within the Design and Access Statement dated 3rd April 2017 by PRC Architecture and Planning approved under planning permission reference: 17/00470/FULM. The approved soft landscaping scheme shall be carried out not later than the first available planting and seeding season after completion of development. Any trees or plants whether new or existing which within a period of five years die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species, or in accordance with details approved by the Local Planning Authority.

Reason: In the interests of the visual appearance of the site and the wider area, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

12. No part of the development shall be occupied until the modified access and egress arrangements from Clarendon Road, as shown in principle on the approved drawings, and the servicing and delivery areas to the front and side of the building, have been completed in full.

Reason: In the interests of the safe operation of the site and the surrounding highway, in accordance with Saved Policies T21 and SE7 of the Watford District Plan 2000.

13. No dwelling within the development shall be occupied until the following facilities have been provided for the use of residents, in accordance with the approved drawings:
- (i) the secure cycle store for at least 168no. cycles;
  - (ii) the bin store for waste and recycling;
  - (iii) the storage cages within the storage rooms on the 1 st-6th floors;
  - (iv) the roof gardens at 7th floor level and on the roof of the tower.

These facilities shall be retained at all times for the use of the residential occupiers of the dwellings.

Reason: To ensure that adequate facilities exist for the future occupiers of the dwellings, in accordance with Saved Policies T10 and SE7 of the Watford District Plan 2000, Policy UD1 of the Watford Local Plan Core Strategy 2006- 31 and the Residential Design Guide 2016.

14. No part of the office floorspace shall be occupied until the following facilities have been provided for the use of employees, in accordance with the approved drawings:
- (i) the secure cycle stores for at least 97 cycles and shower/locker facilities for employees;
  - (ii) the secure cycle stores for at least 24 cycles for visitors;
  - (iii) the bin store for waste and recycling

These facilities shall be retained at all times. Reason: To provide sustainable travel alternatives for employees and visitors, in accordance with Saved Policy T10 of the Watford District Plan 2000 and Policy T3 of the Watford Local Plan Core Strategy 2006-31, and to ensure adequate waste storage in accordance with Saved Policy SE7 of the Watford District Plan 2000.

15. No part of the office floorspace shall be occupied until a detailed Travel Plan for the development, based upon the Hertfordshire County Council document 'Hertfordshire Green Travel Plan Guidance', has been submitted to and approved in writing by the Local Planning. The approved plan shall be implemented as approved at all times, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that the development offers a wide range of travel choices to reduce the impact of travel and transport on the environment, in accordance with Policy T3 of the Watford Local Plan Core Strategy 2006-31.

16. No plant or equipment shall be installed within the roof level plant room until an acoustic assessment has been submitted to and approved in writing by the Local Planning Authority which demonstrates that the sound pressure level from the plant room will be at least 10dB below the lowest LA90 (15 minute) noise level measured at 1m from the adjoining residential flats when all plant and equipment is operational. The assessment shall include appropriate noise mitigation measures. All plant and equipment shall be installed as approved and no plant or equipment shall be brought into operation until the approved mitigation measures have been installed.

Reason: To ensure the operation of the plant and equipment does not give rise to noise nuisance to the residential occupiers.

17. All plant and equipment shall only be sited within the designated plant enclosure shown on the approved drawings. No plant or equipment shall be installed outside the approved plant enclosure unless details have been submitted to and approved in writing by the Local Planning Authority. Details to be submitted for approval shall include siting, size, appearance and technical specifications relating to noise.

Reason: In the interests of the visual appearance of the site and the amenities of the residential occupiers, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

18. No dwelling shall be occupied until details of a communal terrestrial television aerial(s) and satellite dish(es) have been submitted to and approved in writing by the Local Planning Authority.

Reason: In the interests of the character and appearance of the building, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

19. For the avoidance of doubt, no communications development permitted by Class B or Class C of Part 16 of Schedule 2 of the Town and Country Planning (General Permitted Development) (England) Order 2015 shall be undertaken on the building.

Reason: In the interests of the character and appearance of the building, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

20. The cleaning of the building shall only be undertaken using a monorail and cradle access system with the cradle stored at ground level, in accordance with the submitted details, unless otherwise agreed in writing by the Local Planning Authority.

Reason: In the interests of the appearance of the building, in accordance with Policy UD1 of the Watford Local Plan Core Strategy 2006-31.

21. The ground floor commercial unit adjoining the office entrance (eastern unit) shall only be used as a café/restaurant within Class E of the Town and Country Planning (Use Classes) Order 1987 (as amended) and for no other purpose. The use shall not be open to the public before 0800 hours or after 2300 hours on any day.

Reason: The site is located within a designated employment area and in the interests of the functioning and appearance of the site and the amenities of residents within the development.

22. The ground floor commercial unit adjoining the residential entrance (western unit) shall only be used as a café/restaurant within Class E or as offices within Class E of the Town and Country Planning (Use Classes) Order 1987 (as amended) and for no other purposes. The use shall not be open to the public before 0800 hours or after 2300 hours on any day.

Reason: The site is located within a designated employment area and in the interests of the functioning and appearance of the site and the amenities of residents within the development.

23. The commercial unit at 8<sup>th</sup> floor level on the roof of the office building shall only be used as a café/bar within Class E of the Town and Country Planning (Use Classes) Order 1987 (as amended) and for no other purpose.

Prior to commencement of the development the hours of public access to this unit and outdoor terrace shall be submitted to and approved in writing by the Local Planning Authority. The use shall only be operated in accordance with the approved hours for public access.

Reason: The site is located within a designated employment area and to ensure public access to this use in accordance with the planning application.

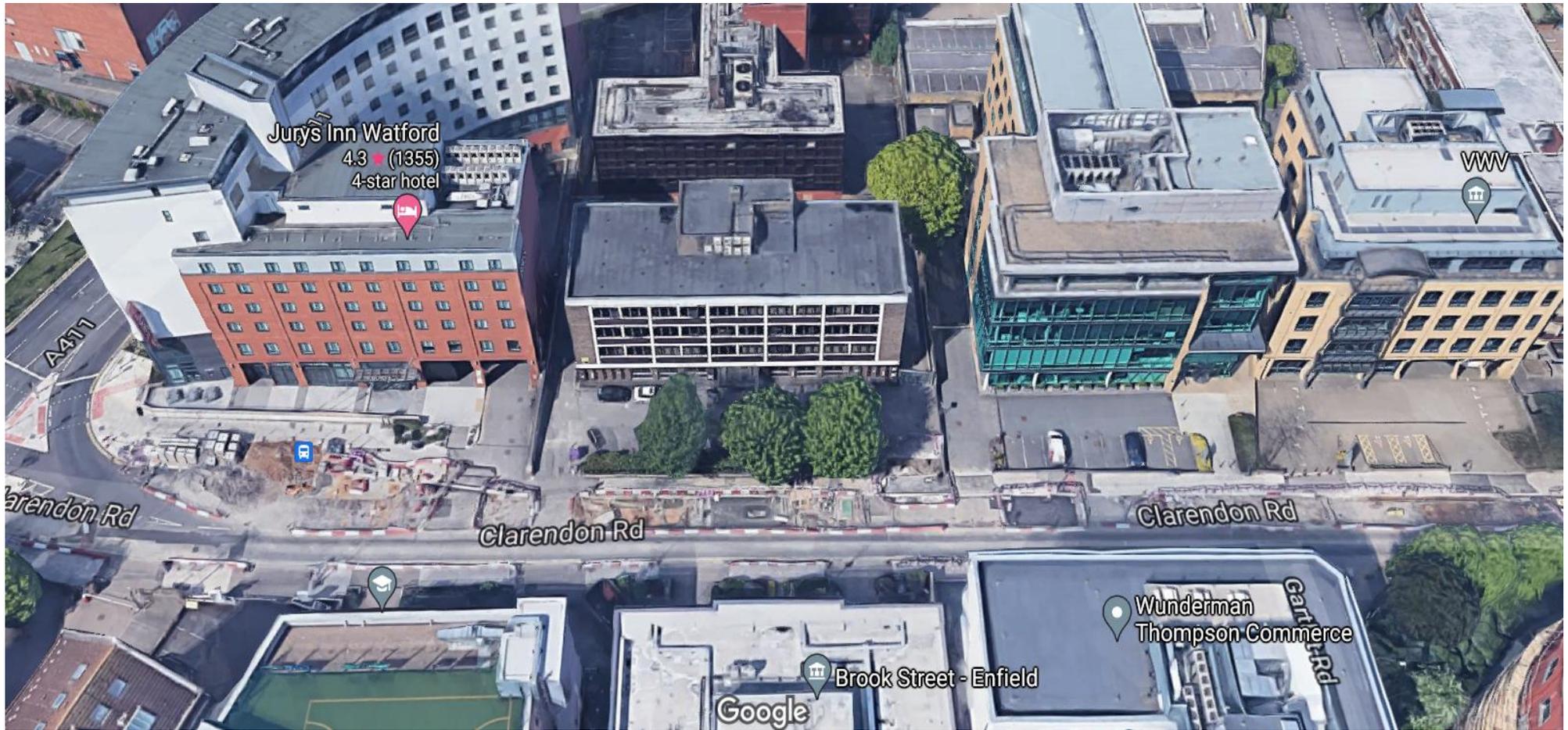
24. The development permitted by this planning permission shall be carried out in full accordance with the approved phasing scheme as set out in Construction Phasing Strategy received 26 February 2021, Phasing Programme received 26

February 2021 and Site Phasing Plan – General Arrangement (ref: 150), as approved under planning reference: 21/00281/DISCON.

Reason: The site is located within a designated employment area and to ensure the office building is constructed at the same time as the residential building.

### Informatives

1. IN907 - Positive and proactive statement - GRANT
2. IN910 - Building Regulations
3. IN912 - Hours of Construction
4. IN909 - Street Name and Numbering
5. IN913 - Community Infrastructure Level Liability
6. HIGH – Storage of Materials – Highway
7. OBSHIG – Obstruction of the Highway
8. MUD – Mud on highway



**Aerial View of Application Site – Google Earth**



CGI as approved under planning permission reference: 17/00430/FULM



CGI as proposed under application reference: 21/00934/VARM

**Comparison CGIs – Consented (Left side image) and Proposed (Right side Image)**



CGI as approved under planning permission reference: 17/00430/FULM



CGI as proposed under application reference: 21/00934/VARM

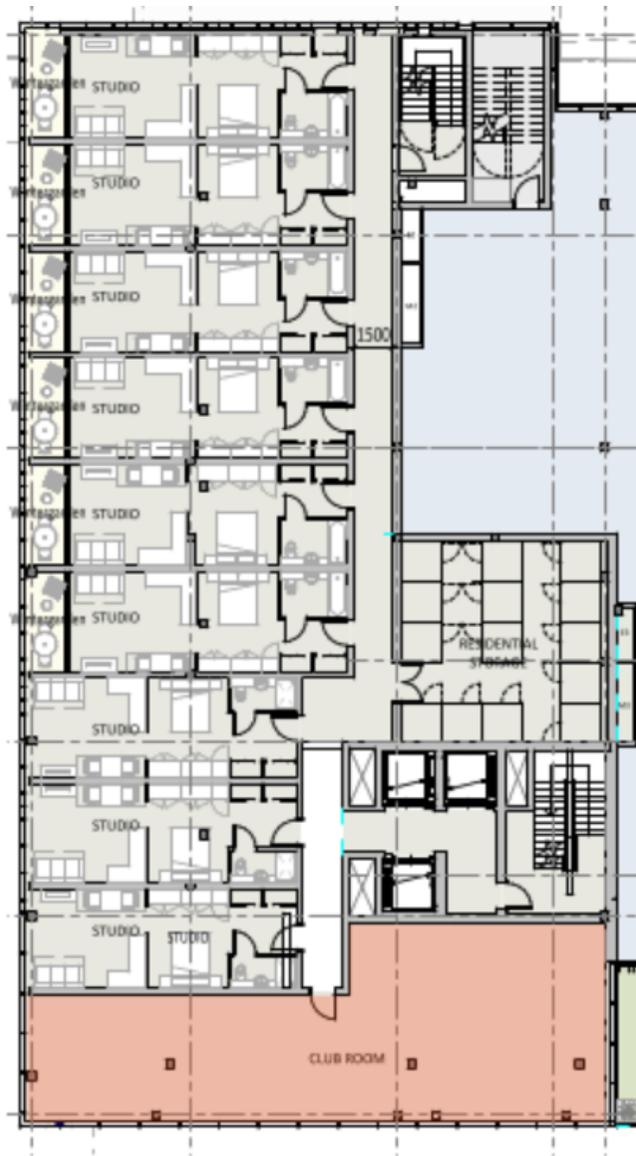


### Key Materials

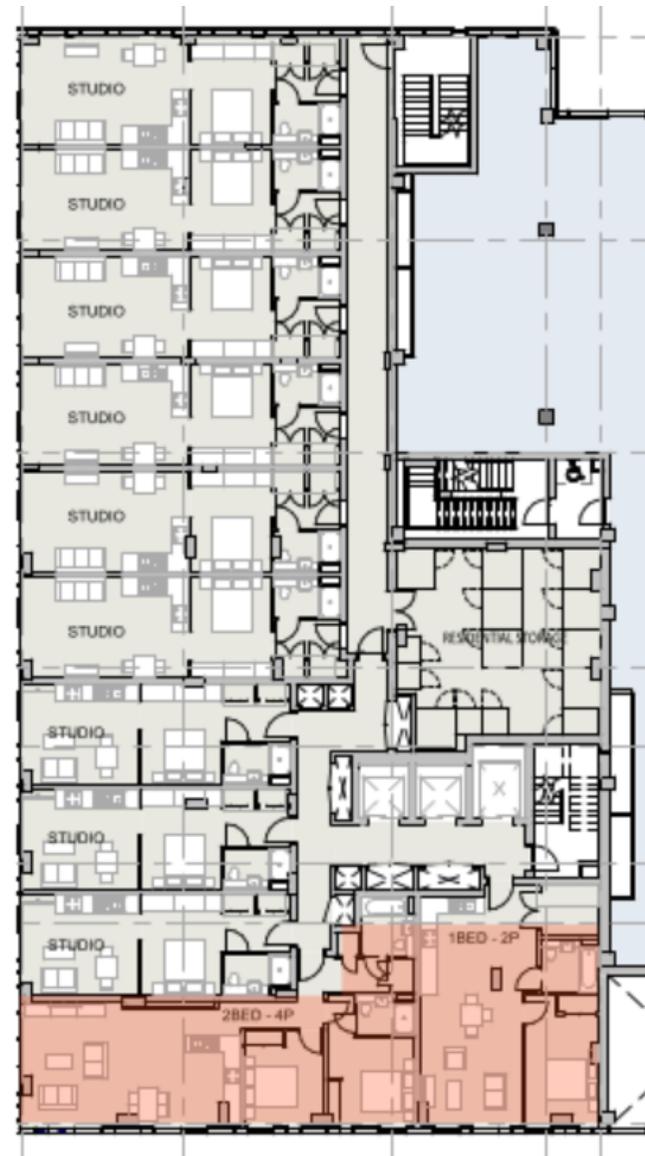
	Consented Material	Proposed Material
1	Recessed channel at floor level - 300 mm deep Aluminium polyester powder coated to match framing	Recessed channel at floor level - 450 mm deep Aluminium polyester powder coated to match framing
2	Glazed spandrel panel	Polyester Powder Coated Aluminium spandrel panel
3	Ventilation louvers above windows glass finish	Ventilation integrated in recessed channel
4	Timber or similar fins to café/ restaurant area	Aluminium polyester powder coated to match framing
5	Openable window. Double glazed unit. Polyester Powder Coated Aluminium Framing System	
6	Glass balustrade to sliding door - balustrade on the outside	



***Comparison Building Heights and Elevations – Consented (Left side image) and Proposed (Right side Image)***

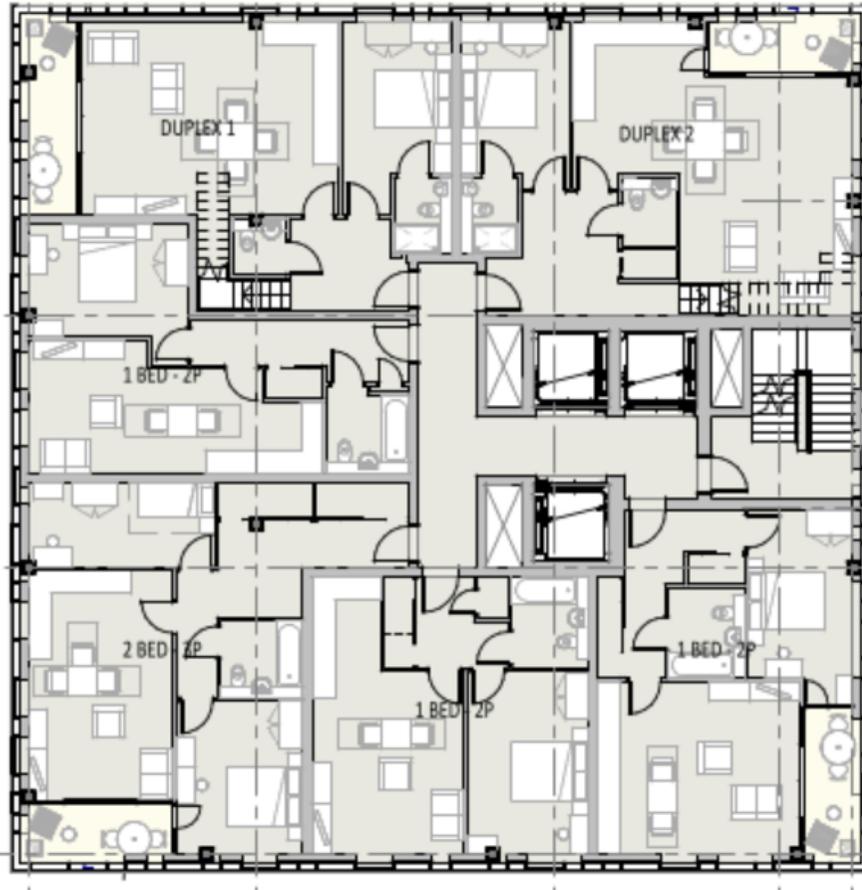


Consented 1st Floor Plan

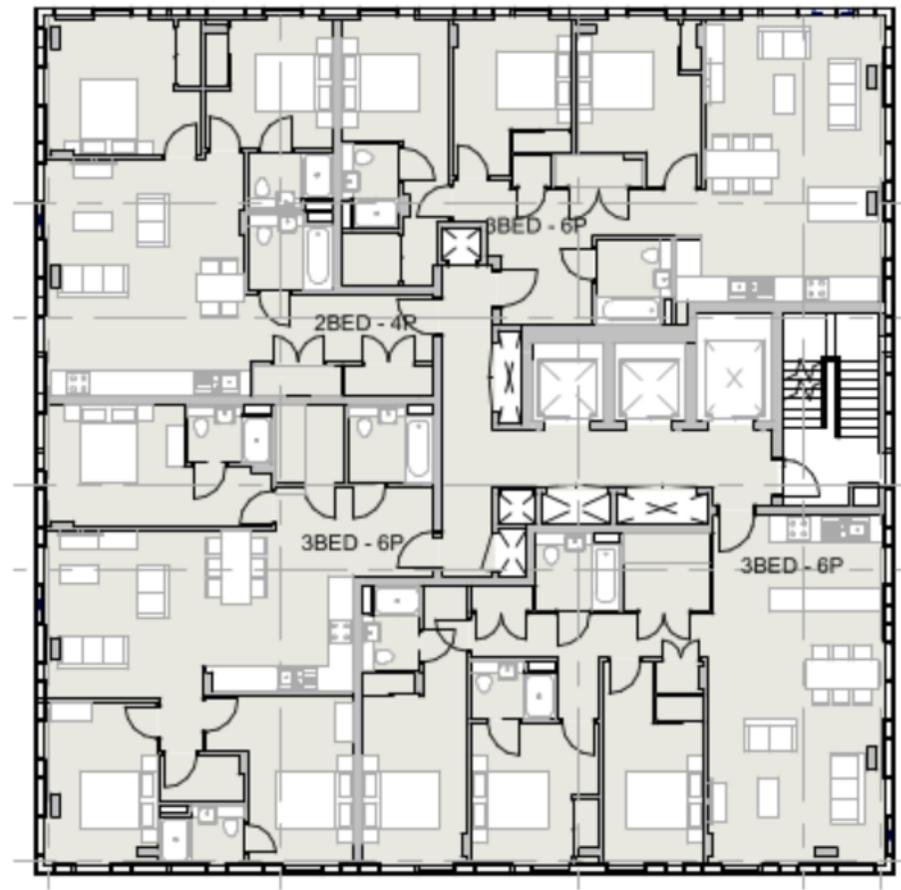


Proposed 1st Floor Plan

**Comparison First Floor Plan – Consented (Left side image) and Proposed (Right side Image)**

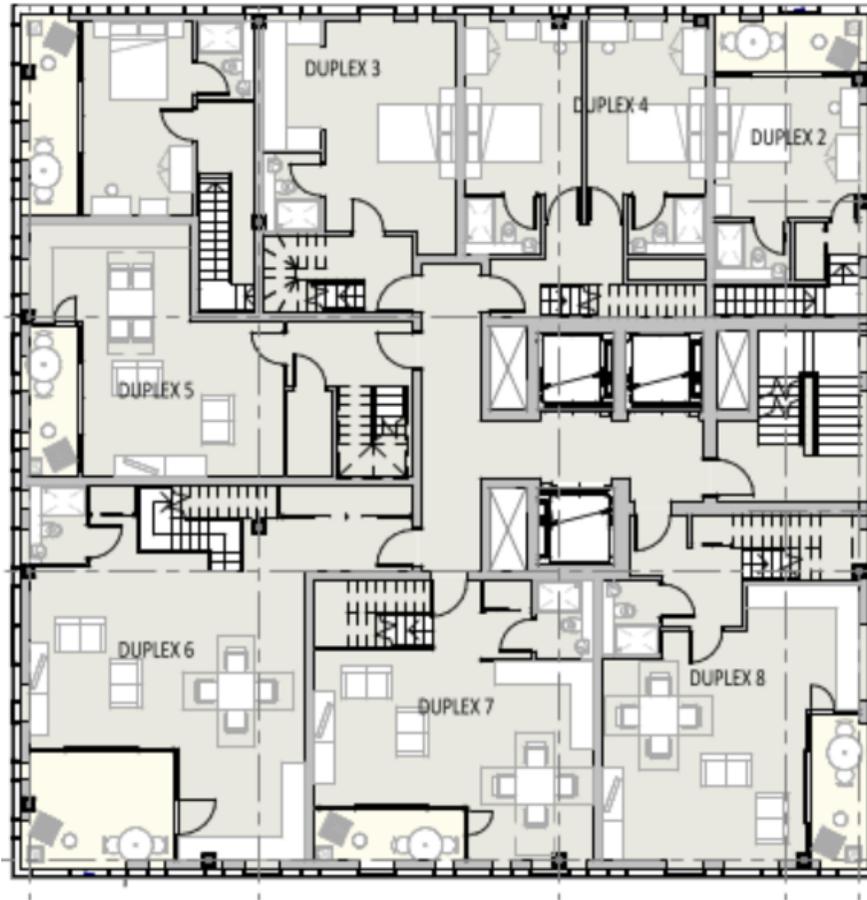


Consented Floor Plan - Former Level 20



Proposed Floor Plan - New Level 22

***Comparison (former) Level 20 (now) Level 22 – Consented (Left side image) and Proposed (Right side Image)***

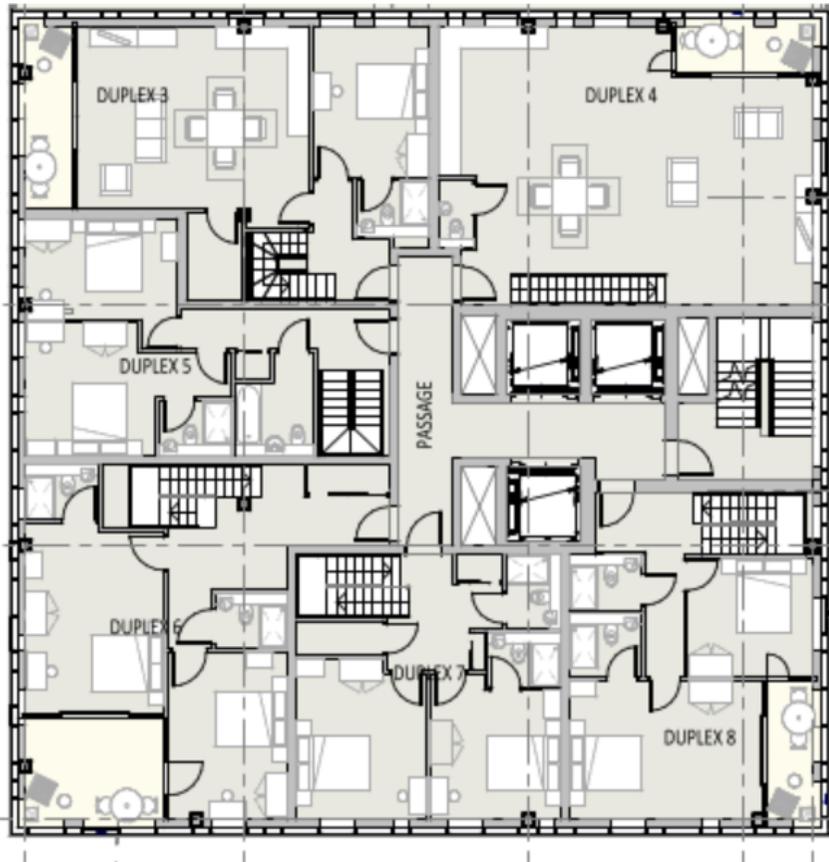


Consented Floor Plan - Former Level 21

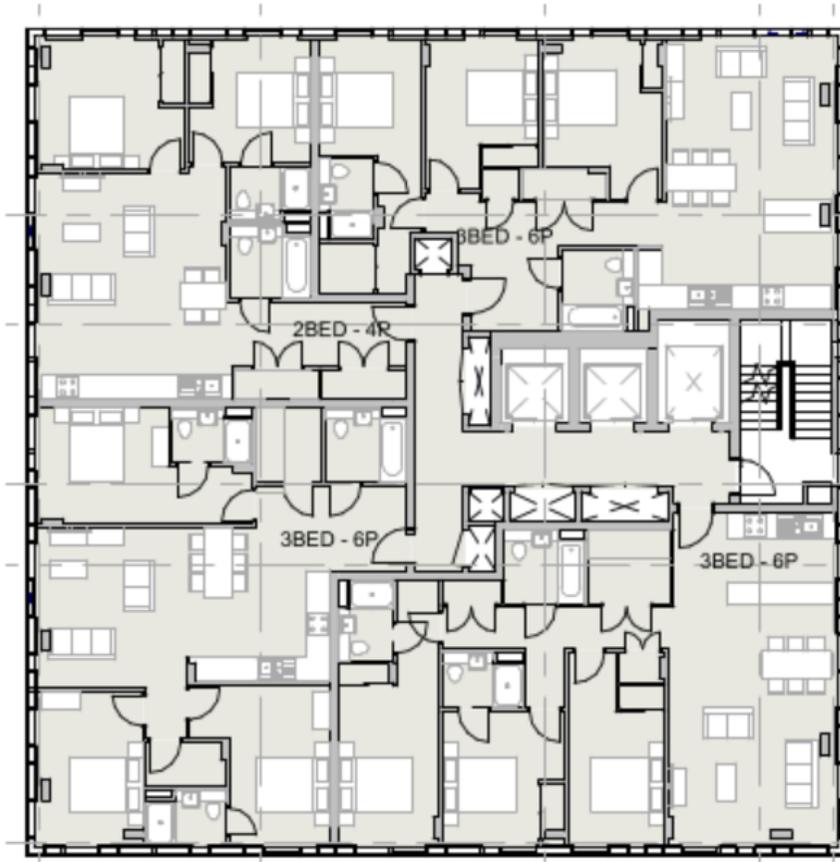


Proposed Floor Plan - New Level 23

**Comparison (former) Level 21 (now) Level 23 – Consented (Left side image) and Proposed (Right side Image)**



Consented Floor Plan - Former Level 22



Proposed Floor Plan - New Level 24

***Comparison (former) Level 22 (now) Level 24 – Consented (Left side image) and Proposed (Right side Image)***

Level	Consented No. of units	Proposed No. of units
Level 20	6	4
Level 21	6	4
Level 22	0	4
Total	12	12